

# Main Street

## LOCAL DEVELOPMENT PLAN



**APPROVAL**

This Local Development Plan has been approved by the City of Stirling under Schedule 2, Part 6, Clause 52 of the Planning and Development (Local Planning Schemes) Regulations 2015.

Signature \_\_\_\_\_  
Date \_\_\_\_\_

---

WAS APPROVED BY RESOLUTION OF THE WESTERN AUSTRALIAN PLANNING COMMISSION  
ON

.....  
CHAIRPERSON, WESTERN AUSTRALIAN  
PLANNING COMMISSION

DATE OF EXPIRY

## CONTENTS

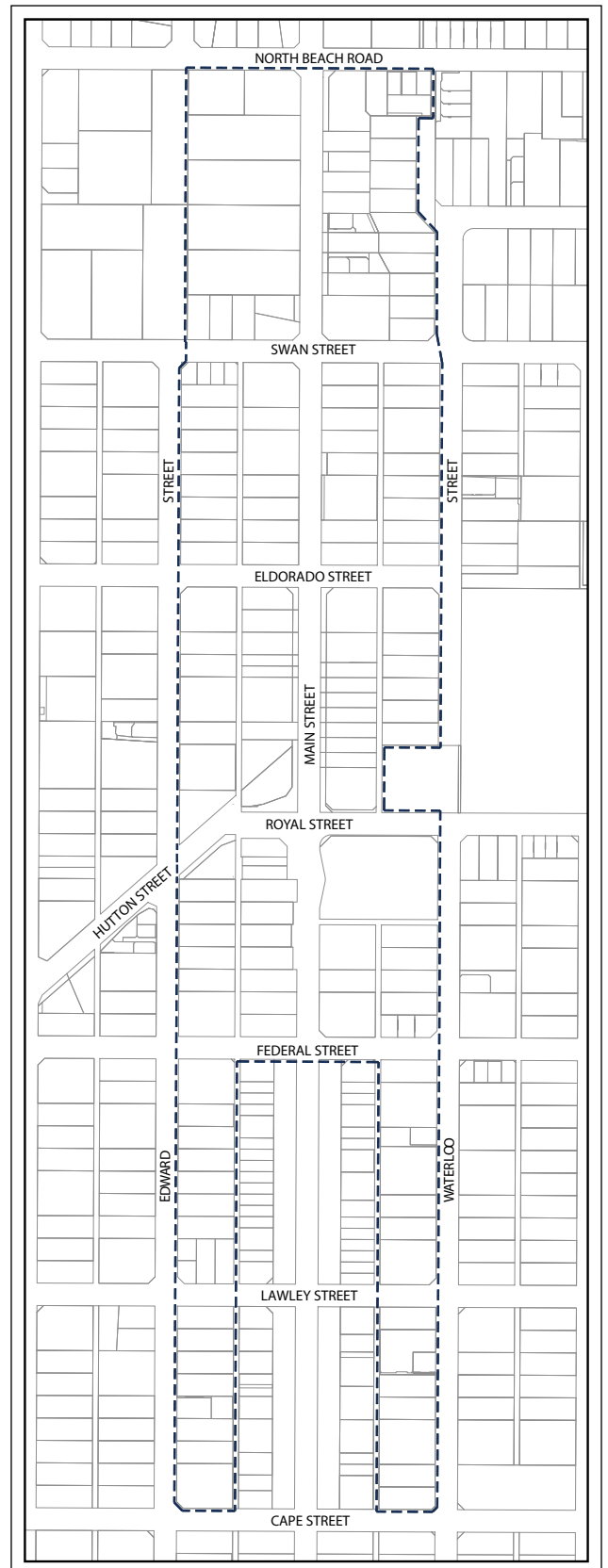
1.0 USING THIS DOCUMENT.....	4
2.0 IMPLEMENTATION.....	5
3.0 GENERAL DEVELOPMENT PROVISIONS	7
4.0 STREET TYPES.....	13
5.0 BUILDING TYPES .....	16

# 1.0 USING THIS DOCUMENT

This Local Development Plan (LDP) provides the primary controls for new development in the Main Street Local Development Plan, **Figure 1**.

The Local Development Plan is divided into 5 sections as outlined below:

- Section 1 - Using this document - outlines the structure of the document.
- Section 2 - Implementation outlines how this document is to be used in relation to other planning documents.
- Section 3 - General Development Provisions Outlines the general development provisions for all lots.
- Section 4 - Street Types - outlines the details of new streets that have to be constructed and existing streets that require upgrading.
- Section 5 - Building Types - outlines 4 building types that define the height and placement of buildings, location of open space and parking.



**Figure 1** - Local Development Plan Area

# 2.0 IMPLEMENTATION

## 2.1 Operation

This Local Development Plan (LDP) has been prepared to guide development within the Main Street District Centre. It will be used by the City in the assessment of development and subdivision applications.

All development within this Local Development Plan is subject to the provisions of this LDP. Where there is an inconsistency between the general development provisions and the specific development provisions, the specific development provisions shall prevail.

## 2.2 Relationship to Scheme and Policies

This Local Development Plan is to be read in conjunction with the City's Local Planning Scheme, Local Planning Policies as well as the Residential Design Codes.

Where there is an inconsistency between the provisions of this Local Development Plan and adopted Policies, the provisions of this LDP shall prevail.

Where the Local Planning Scheme, City Policies and/or Local Development Plan do not apply; the provisions of the Residential Design Codes apply.

The Main Street Urban Design Strategy should also be read for more detailed information about the intent for the area.

## 2.3 Residential Design Codes

- a) In accordance with the Local Planning Scheme, the Residential Density Code applicable to the LDP area is R-AC0.
- b) The requirements of the SPP 7.3 Residential Design Codes Volumes 1 & 2 are applicable to the relevant residential component of developments, except for where the provisions are replaced or amended as identified in Section 3.0 of this LDP.

## 2.4 Vision

The vision is to transform the area to a mixed use multi storey main street with a continuous building edge in a landscaped setting. Buildings will have retail uses on the ground floor with residential buildings above. In the residential areas the vision is for mid-rise apartments in a landscaped setting.

## 2.5 Objectives

- a) To create a lively and dynamic activity centre with a varied mix of housing, employment and retail activities;
- b) To encourage built form that demonstrates design excellence and creates a diverse, fine-grain urban character while providing landscaped amenity;
- c) To sensitively integrate and transition new development into the surrounding residential neighbourhood;
- d) To provide a high level of pedestrian amenity and safety through the provision of continuous active retail streetscapes and passive surveillance;
- e) To create a new civic space and public plaza/s that are safe, attractive and surrounded by active uses that will become the focal point of the centre;
- f) To set buildings back from Main Street to enable future ceding of this land for public realm enhancements such as wider footpaths and street trees;
- g) To control building heights and plot ratio to ensure the desired character and proportions of the streets;
- h) To encourage built form which is designed to achieve exemplary environmentally sustainable outcomes incorporating innovative design, construction and management principles; and

# 2.0 IMPLEMENTATION

- i) To ensure that development provides sufficient on site parking for the needs of all residents, visitors, staff and customers.

## 2.6 Variations

Proposed variations to this Local Development Plan will be considered in accordance with the objectives of this LDP and the applicable Design Principles, Design Guidance, Acceptable Outcomes and Element Objectives of the Residential Design Codes.

## 2.7 Definitions

Unless otherwise defined, definitions in this document shall be as per the Residential Design Codes or the Local Planning Scheme.

*Attached Townhouse* means a two storey single house or grouped dwelling with shared walls to one or more dwellings and parking accessed from a rear Right of Way.

*Base Building Standard* means an as-of-right development entitlement applicable to lots within the Local Development Plan area.

*Bonus Building Standard* means a development entitlement additional to the base building standard applicable to lots within the Local Development Plan area that is contingent on eligibility requirements being satisfied. These bonuses are expressed as bonus plot ratio and bonus height.

*Civic Space* means a publically owned space accessible for common use by the public.

*Design Excellence* is measured against the 10 design principles outlined in State Planning Policy 7.0.

*Public Plaza* means a plaza, that is owned privately, but is always accessible to the public by way of easement or other legal agreement.

*Road Widening Area* means an area of a lot not occupied by any building and set aside for the purpose of widening a road reserve.

*Substantial Redevelopment* means a development application which proposes the complete redevelopment of a site including demolition of existing buildings.



# 3.0 GENERAL DEVELOPMENT PROVISIONS

## 3.1 Road Widening

Development on the following sites shall not encroach into the Road Widening Area shown on **Figure 9**:

- Lot 508, HN 199 Main Street
- Lot 509, HN 201 Main Street
- Lot 510, HN 203 Main Street
- Lot 514, HN 217 Main Street
- Lot 2, HN 196 Main Street
- Lot 324, HN 200 Main Street
- Lot 80, HN 212 Main Street
- Lot 800, HN 216 Main Street

Development shall be setback from the Road Widening Area as if it was the lot boundary.

The portion of a lot within the Road Widening Area may be included in the total lot area for the purpose of assessing development against the requirements of this Local Development Plan.

## 3.2 Public Plazas

These provisions are in addition to Element 2.8 – Development Incentives for Community Benefit of the R - Codes (Volume 2).

- a) New public plazas may be provided on sites zoned District Centre. Public plazas shall:
  - i) Have a minimum dimension of 7 metres and not be less than 75m<sup>2</sup> in area;
  - ii) Be publically accessible at all times;
  - iii) be a minimum 80% uncovered;
  - iv) Have activated non-residential uses on the ground floor fronting the public plaza; and
  - v) Be landscaped to the satisfaction of the City of Stirling.
- b) Public Plazas are to be privately owned. An easement will be required to ensure public access. The land area of the Public Plaza may be used in the calculation of plot ratio and street setbacks for development.

## 3.3 Civic Space

- a) A mandatory civic space shall be provided at the existing public car park at Lot 340, HN 223 and Lot 4, HN 223A Main Street.
- b) The civic space is to be designed to comply with the following design principles, refer concept design **Figure 2**:
  - i) A multi-generational space that has space for performances.
  - ii) Naturally shaded for enjoyment all year round.
  - iii) Filled with greenery and places to sit.
  - iv) Buffered from surrounding roads.
  - v) Activated by the ability to accommodate up to 50 market stalls.
  - vi) Ground floor activated uses to surround the space.
  - vii) Leaving sufficient space for future multi storey, mixed use development that incorporates public car bays equal to that currently provided on the site.
- c) The civic space shall have a minimum size of 1200m<sup>2</sup> and minimum dimension of 15m.

## 3.4 Landscaping and Open Space

These provisions are in addition to Clause 5.3.2 of the R - Codes (Volume 1) and Clause 3.3 – Tree Canopy and Deep Soil Areas, Clause 3.4 – Communal Open Space and Clause 4.12 – Landscape Design of the R - Codes (Volume 2).

- a) Deep Soil Zones shall be placed in the locations outlined in the Building types.
- b) A Landscaping Plan shall be submitted for all developments with 5 or more dwellings in accordance with the Local Planning Policy 6.6: Landscaping.
- c) All Deep Soil Areas shall include trees (min. 90 litre at planting) at a minimum 5m spacing.

# 3.0 GENERAL DEVELOPMENT PROVISIONS



**Figure 2** - Civic Space Concept Design



# 3.0 GENERAL DEVELOPMENT PROVISIONS

## 3.5 Vehicle Access

These provisions replace Clause 5.3.5 – Vehicular Access of the R - Codes (Volume 1) and Clause 3.8 – Vehicle Access of the R - Codes (Volume 2).

- a) No vehicle access shall be permitted from any street where a site has alternative access via a ROW.
- b) No more than one vehicle access point for any one site shall be permitted from a street other than a ROW.
- c) Notwithstanding sub-clauses a and b, the substantial redevelopment of Lot 800, HN 216 Main Street shall be permitted the following vehicle access arrangements:
  - i) Main Street – no access/egress permitted;
  - ii) Royal Street – maximum one access/egress point permitted;
  - iii) Waterloo Street – maximum two access/egress points permitted; and
  - iv) Garner Lane – no limit on access/egress.
- d) Maximum crossover widths shall be:
  - i) 3.0 metres where serving up to 15 dwellings; or
  - ii) 6 metres where serving 15 dwellings or more.
- e) Applications for development approval are to be accompanied by details on how vehicular and pedestrian access and egress will be managed on adjoining sites to ensure that access is not restricted to any site.
- f) No further closure of median island openings and associated access points shall be undertaken by the City unless consultation with the affected owners is undertaken.

## 3.6 Building Outlook

These provisions are in addition to Clause 5.2.3 - Street Surveillance of the R - Codes (Volume 1) and Element 3.6 – Public Domain Interface of the R - Codes (Volume 2).

- a) All habitable rooms shall have a major opening.
- b) Each dwelling shall have habitable rooms facing all streets, Rights of Ways and Public Access Ways.

## 3.7 Public Domain Interface

These provisions replace Acceptable Outcomes A3.6.2 of the R- Codes (Volume 2) at Element 3.6 for multiple dwellings. These provisions replace Deemed-to-Comply provisions C1.1-C1.2 and C1.4-C1.5 of the R-Codes Volume 1 Element 5.2.1 for single houses and grouped dwellings.

- a) All development shall have either residential or non-residential uses on the ground floor fronting a street. Car parking is to be sleeved behind land uses as identified in **Figures 3, 4 and 5**.
- b) At grade car parking is not permitted to front a street or public access way at the ground level.
- c) A residential building's ground level elevation may be 1.2m above natural ground level at the street level, **Figure 5**.
- d) Non residential buildings shall be level with the street.
- e) The ground level of a mixed-use building with a frontage to Main Street shall include a continuous solid canopy for pedestrian shelter.
- f) Where a property has frontage to Main Street that shall be their primary street frontage.

# 3.0 GENERAL DEVELOPMENT PROVISIONS

## 3.8 Non-Residential and Mixed-Use Development

- a) In accordance with Table 1 of Local Planning Scheme No. 3 residential development is not permitted on the ground floor fronting a road reserve greater than 10m in width in the District Centre Zone.
- b) Non - Residential and Mixed-Use development shall be designed in accordance with Local Planning Policy 4.2: Mixed Use and Commercial Centre Design Guidelines. The provisions of this LDP shall prevail where inconsistent with this Policy.

## 3.9 Residential Development

The residential zoning of this land permits single houses, grouped dwellings and multiple dwellings.

All building types would allow the construction of any type of dwelling within the setback / height / plot ratio constraints.

## 3.10 Building Heights and Plot Ratio

- a) The relevant maximum building height and plot ratio for each building type is outlined in Section 5.0 Building Types.
- b) In accordance with clause 6.18.6 of Local Planning Scheme No. 3 the minimum building height is 2 storeys for substantial redevelopment.
- c) In accordance with clause 6.18.5 of Local Planning Scheme No. 3 the minimum residential plot ratio is 0.5 for substantial redevelopment.

## 3.11 Development Bonuses

Bonus Buildings Standards above the Base Building Standards may be applicable to lots only zoned District Centre where they meet the Development Bonus Criteria as outlined in Table 1. The extent of Bonus Building Standards that may be applicable (based on achieving the Development Bonus Criteria in Table 1) is outlined in Table 2.

Development Bonus Criteria	Applicability	Bonus Points
Residential Development Provided	15 – 25 Dwellings Provided	0.5
	26 or more Dwellings Provided	1.0
Road Widening Ceded	<100m <sup>2</sup>	0.5
	>100m <sup>2</sup>	1.0
Public Plaza / Civic Space Provided	<200m <sup>2</sup>	0.5
	>200m <sup>2</sup>	1.0
<b>Maximum Achievable Points</b>		<b>3.0</b>

Table 1 - Criteria for Bonus Points

Bonus Points Achieved	Height Bonus (storeys)	Bonus Plot Ratio
0.5	0	0.15
1.0	1	0.3
1.5	1	0.45
2.0	2	0.6
2.5	2	0.75
3.0	3	0.9

Table 2 - Bonus Building Standards

# 3.0 GENERAL DEVELOPMENT PROVISIONS

## 3.12 Parking

Parking provisions are in addition to Clause 5.3.3 – Parking and Clause 5.3.4 – Design of Car Parking Spaces of the R - Codes (Volume 1) and Element 3.9 – Car and Bicycle Parking of the R - Codes (Volume 2).

- a) Parking areas shall be designed in accordance with **Figure 3, 4 and 5** and the Specific Development Provisions.
- b) Car parking shall be provided on site in accordance with **Table 3** and is a mandatory standard and replaces the parking requirements listed in Table 1 of Local Planning Policy 6.7.
- c) Parking areas shall be designed having regard to **Figures 3, 4 and 5**. Basement parking is likely to be required to achieve an acceptable compliant development outcome.
- d) The parking reductions provided for in Local Planning Policy 6.7 shall not apply.
- e) An alfresco area does not require any additional parking spaces.
- f) Existing or new on-street parking directly adjacent to the site may be calculated in the total.
- g) The parking provisions in **Table 3** only apply to new development.
- h) Change of use applications are exempt from providing any additional car parking on site.
- i) Substantial redevelopment of Lot 340, HN 223 and Lot 4, HN 223A Main Street shall be required to incorporate public car parking bays to replace the existing bays, in addition to any parking required to service any new land uses on the land.

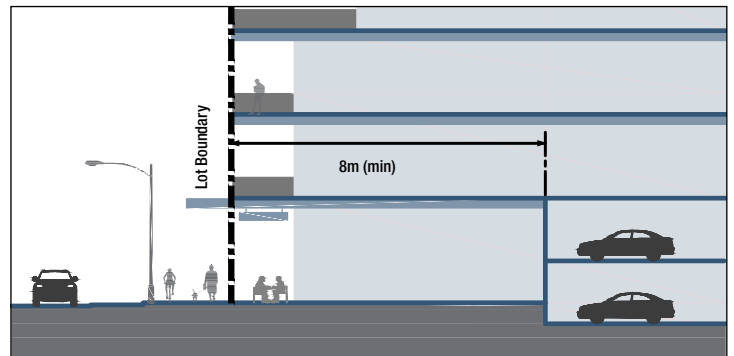


Figure 3 - At grade parking, non-residential ground floor use

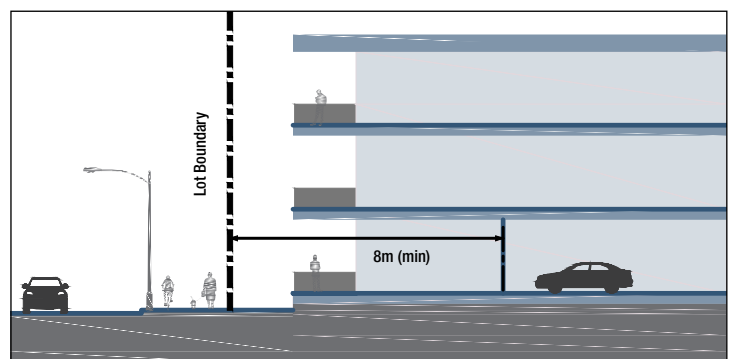


Figure 4 - At grade parking, residential ground floor use parking,

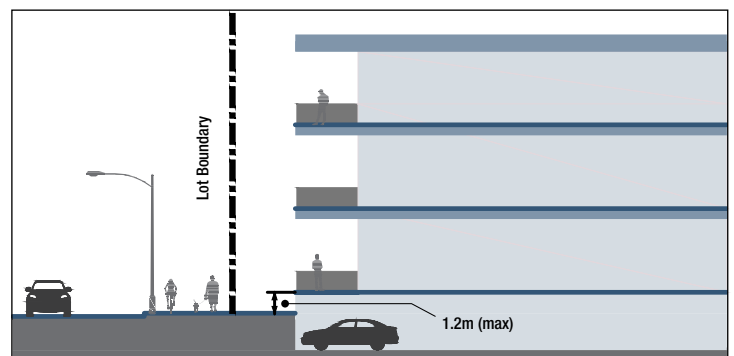


Figure 5 - Below grade residential use

Land Use	Required Spaces
Non-residential land use	3.5 per 100m <sup>2</sup> NLA
Shop (1000m <sup>2</sup> and above)	5.0 per 100m <sup>2</sup> NLA
Hotel, Tavern and Small Bar	6.0 per 100m <sup>2</sup> NLA
Residential	As per R-Codes

Table 3 - Car Parking Standards

## 3.0 GENERAL DEVELOPMENT PROVISIONS

### 3.13 Staging

As a result of the shared vehicle access and car parking arrangements which currently exist through the centre, any substantial redevelopment will require consideration of the impact on adjoining properties.

The following guidance is provided in such scenarios:

- a) Proponents shall consult with affected adjoining landowners where any existing common access arrangements are proposed to be changed as a result of redevelopment.
- b) Proponents shall be responsible for the required removal and/or relocation of shared vehicle access points which may arise as a result of a proposed development.
- c) Where substantial redevelopment occurs adjoining an existing development, the City shall allow temporary additional signage where it is deemed necessary to ensure continued commercial exposure.

# 4.0 STREET TYPES

## 4.1 Street Types

This Local Development Plan contains a number of Street Types - **Figures 6 - 9**:

- a) Street Type 1 is on all streets except Main Street. Street Type 2 and 3 apply to Main Street. Street Type 4 applies to all Right of Ways.
- b) All substantial redevelopment is required to upgrade/construct the adjoining portion of road reserve to the identified Street Type in the Specific Development Provisions as a condition of development, subdivision/built-strata approval.
- c) Satisfactory arrangements (such as cash-in-lieu) may be made by the landowner with the City for the full cost of upgrading/constructing the adjoining portion of the existing road reserve to the standard as required by the relevant Street Type cross-section.
- d) Existing street trees shall not be removed and new street trees shall be planted at a maximum linear distance of 7.0m or between each on-street car parking bay.
- e) New Rights of Way shall be constructed in accordance with the relevant street type.
- f) The upgrade of the Main St, Hutton St and Royal St carriageways will be the responsibility of the City, but any tying in with existing footpaths required as part of substantial redevelopment will be the responsibility of the landowner.

# 4.0 STREET TYPES

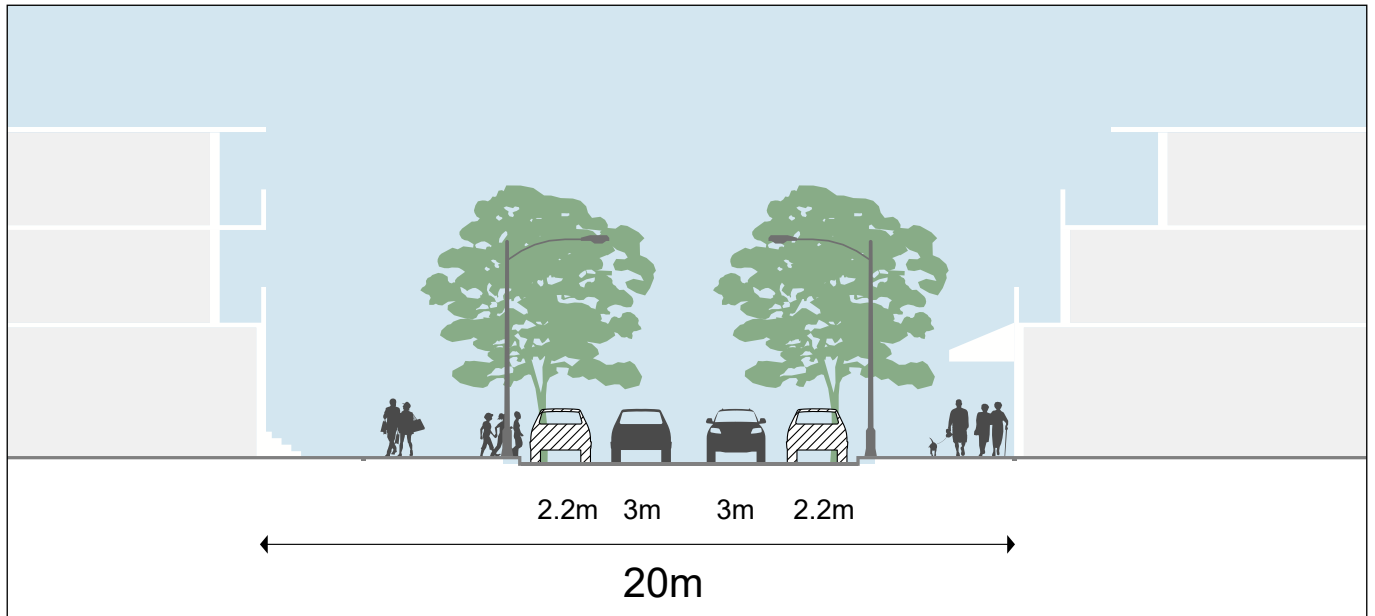


Figure 6 - Street Type 1: Two way traffic movement on local streets with on-street parking (20m)

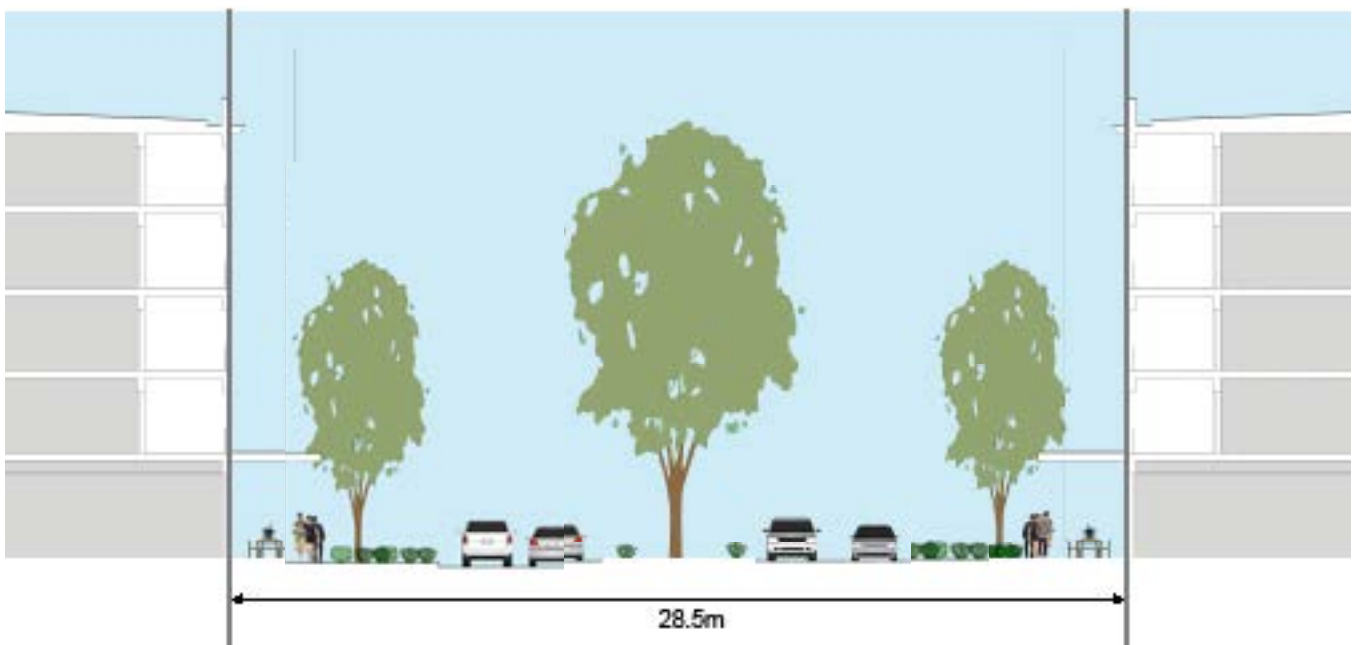


Figure 7 - Street Type 2: Main Street between Federal Street and Hutton Street with long term peak bus lane and off-peak parking lane (28.5m)



# 4.0 STREET TYPES

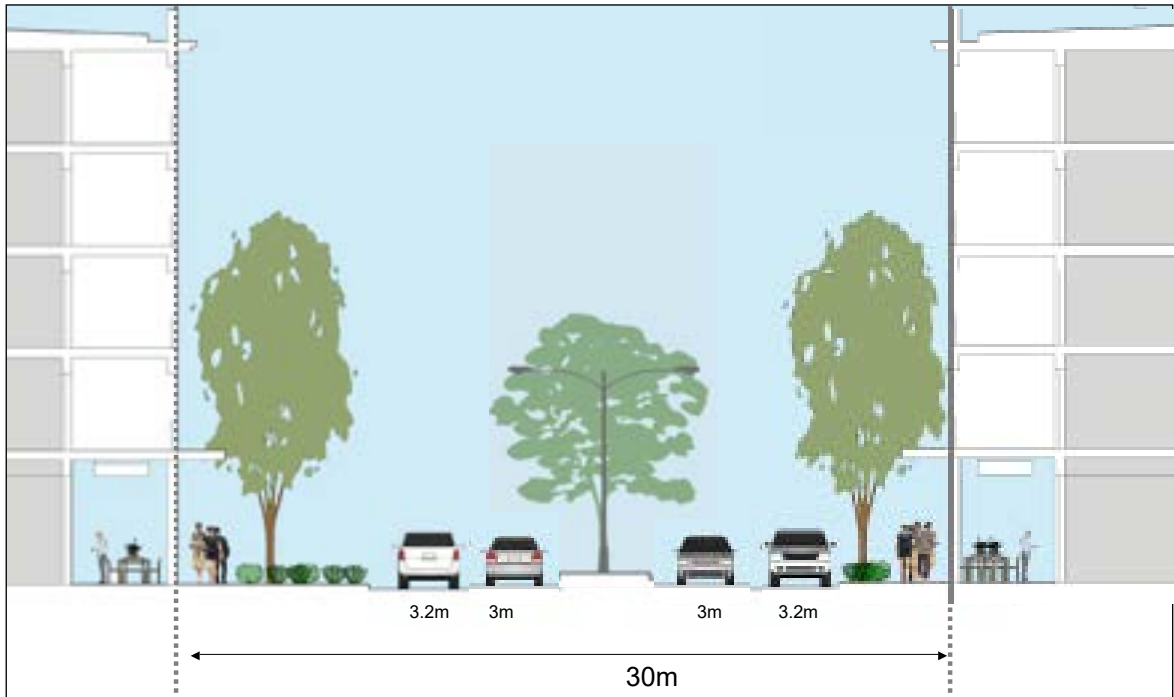


Figure 8 - Street Type 3: Main Street between Eldorado Street and Hutton Street with long term peak bus lane and off-peak parking lane (30m)

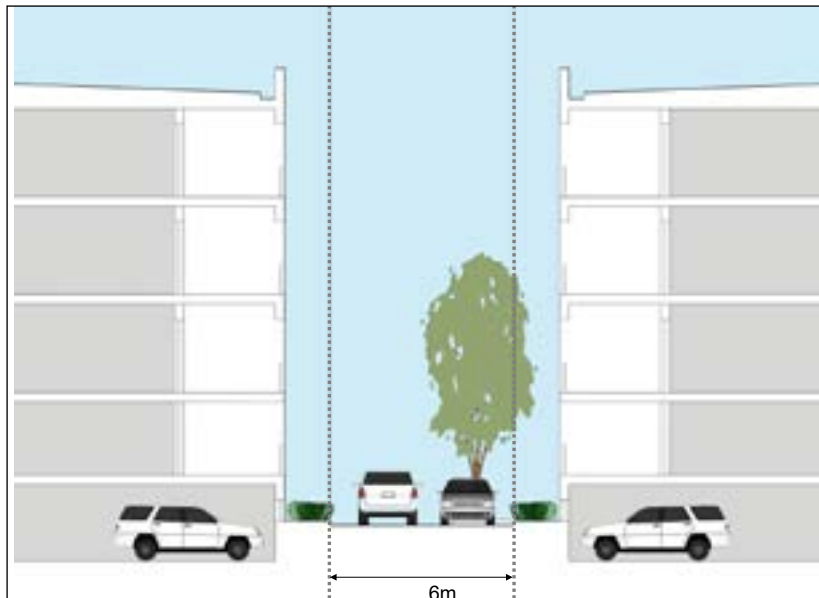


Figure 9 - Street Type 4: Right of Way 6m

# 5.0 BUILDING TYPES

## 5.1 BUILDING TYPES

a) The Local Development Plan is categorised into Detached and Attached Building Types with both residential and non-residential variations.

b) All development and subdivision shall comply with the applicable building type which is illustrated in **Figure 10**.

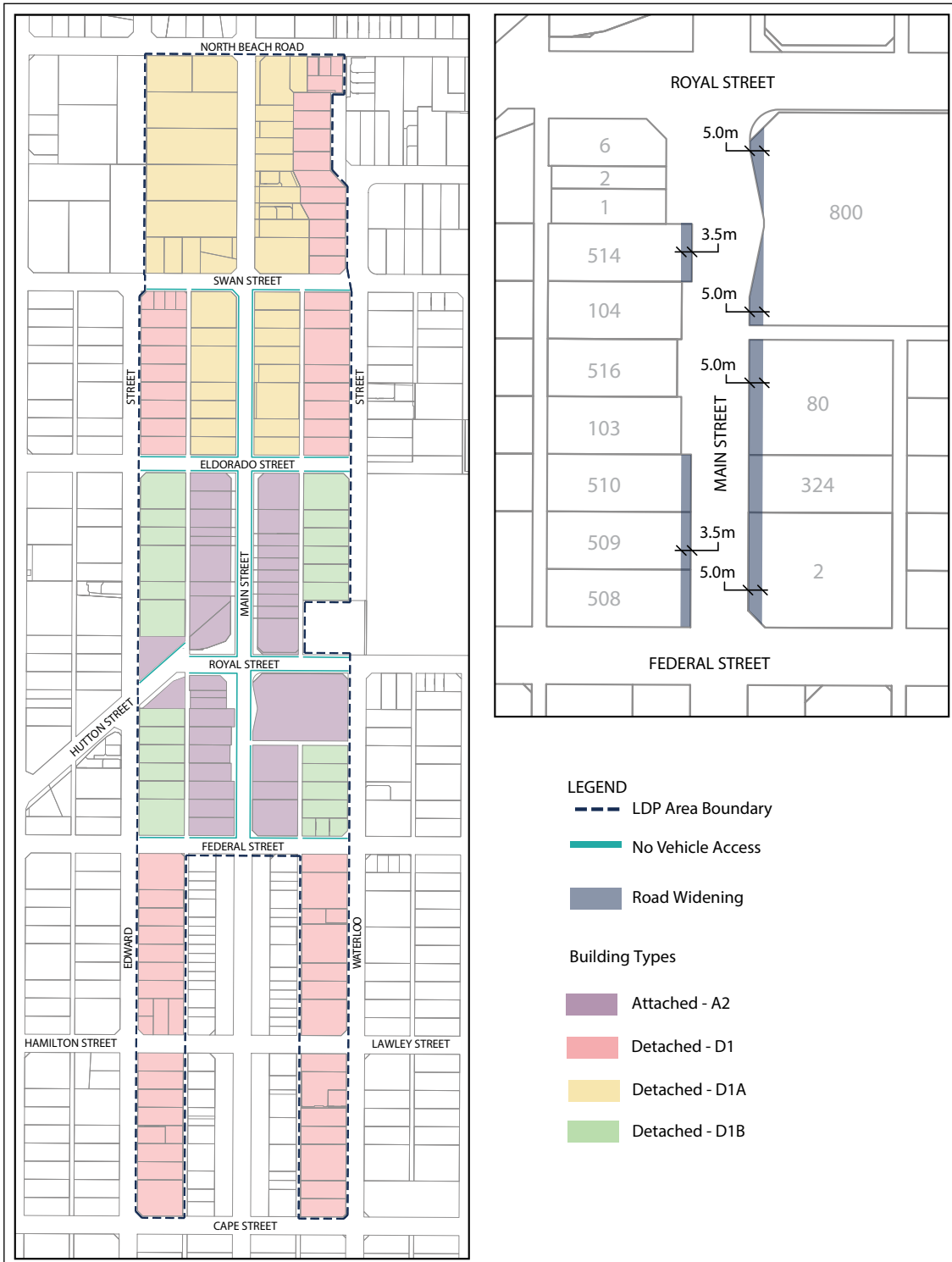


Figure 10 - Building Types & Road Widening Plan

# 5.0 BUILDING TYPES

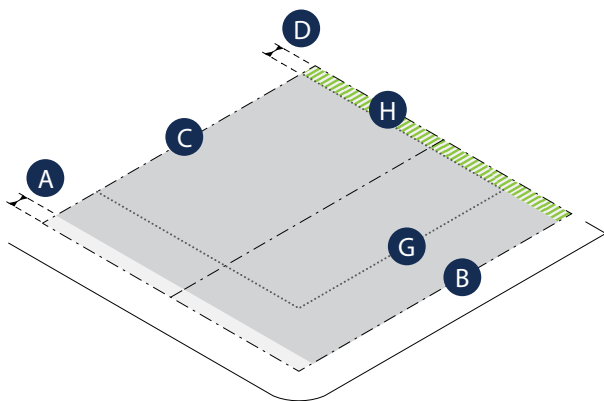
## 5.2 ATTACHED BUILDING TYPES

### 5.2.1 ATTACHED - A2

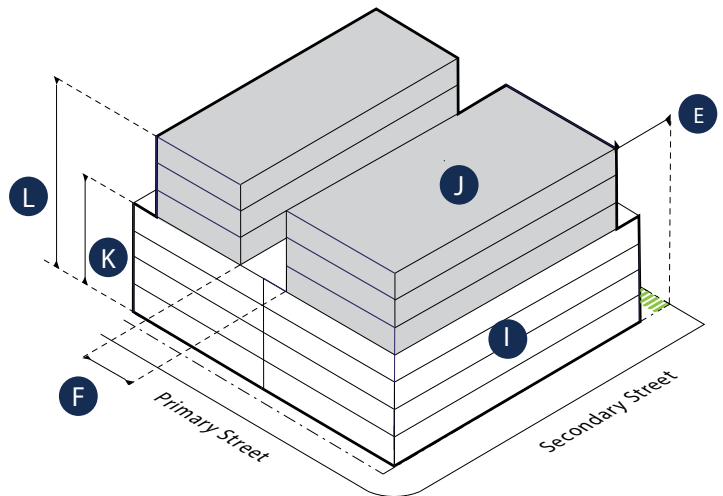
#### A. A2 BUILDING PLACEMENT

A2 types will be located in non-residential zones only. Buildings in the A2 type will be built to the front and side boundaries forming a continuous urban edge. Ground floor shop fronts will face the street and upper floor windows will be oriented to face the street. Amalgamation of lots to create larger sites is encouraged to maximise development potential. Building height is limited to 4 storeys. Development bonuses to permit greater heights and plot ratio are provided.

#### B. A2 - BUILDING SITING



#### C. A2 - BUILDING ENVELOPE



Boundary Setbacks		
<b>A</b>	Primary Street Setback (min/max)	2m / 3m
<b>B</b>	Secondary Street Setback (min)	Nil
<b>C</b>	Side Setback (min)	Nil
<b>D</b>	Rear Setback (min)	3m
Upper Setbacks		
<b>E</b>	Rear, above 4 storeys (min)	9m
<b>F</b>	Side, above 4 storeys (min/max)	3m
Parking Setbacks (min)		
<b>G</b>	Street Setback	8m
	Rear Setback	2m
Landscaping Zones, Depth (min)		
<b>H</b>	Rear	3m

Plot Ratio (Max)		
<b>I</b>	Base Plot Ratio	2.5
<b>J</b>	Bonus Plot Ratio	*
Building Height, Storeys (max)		
<b>K</b>	Base Building Height	4
<b>L</b>	Bonus Building Height	*

\*Bonus Building Standards are provided for in clause 3.11

# 5.0 BUILDING TYPES

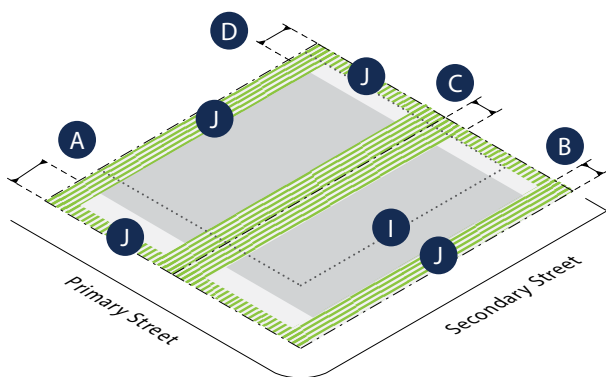
## 5.3 DETACHED BUILDING TYPES

### 5.3.1 DETACHED - D1

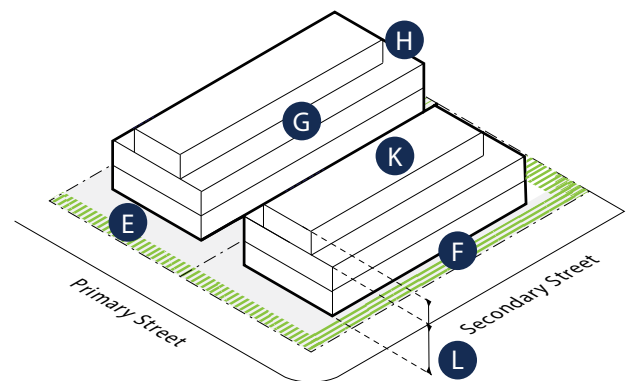
#### A. D1 BUILDING PLACEMENT

D1 types will be located in residential zones only. Apartment buildings will be separated by landscaping between lots to maintain the garden suburb feeling. Generous front setbacks are to enable ground floor courtyards and space for trees to achieve canopy coverage. Windows will face the street. Rear setbacks are to allow for trees and for rear facing apartments to have a landscaped outlook or an outlook onto rights of ways. Amalgamation of lots to create larger sites is encouraged to maximise development potential. Base building height is limited to 2 storeys and maximum heights with a bonus are limited at 3 storeys.

#### B. D1 - BUILDING SITING



#### C. D1 - BUILDING ENVELOPE



Lower Levels Boundary Setbacks (min)	
<b>A</b>	Primary Street Setback Area 6m
<b>B</b>	Secondary Street Setback Area 3m
<b>C</b>	Side Setback Areas 3m
<b>D</b>	Rear Setback 3m
Upper Levels Boundary Setbacks - Above 2 Storeys (min)	
<b>E</b>	Primary Street Setback 6m
<b>F</b>	Secondary Street Setback 3m
<b>G</b>	Side Setback 6m
<b>H</b>	Rear Setback 9m
Parking Setbacks (min)	
<b>I</b>	Street Setback 8m
	Rear Setback 3m

Deep Soil Areas, Width (min)	
<b>J</b>	Rear 3m
	Front 3m
	Side (min only one side required) 3m
Plot Ratio (max)	
<b>K</b>	Lots less than 1000m <sup>2</sup> and/or less than 24m frontage 0.8
	Lots greater than 1000m <sup>2</sup> and greater than 24m frontage 1.1
Building Height, Storeys (max)	
<b>L</b>	Lots less than 1000m <sup>2</sup> and/or less than 24m frontage 2
	Lots greater than 1000m <sup>2</sup> and greater than 24m frontage 3

# 5.0 BUILDING TYPES

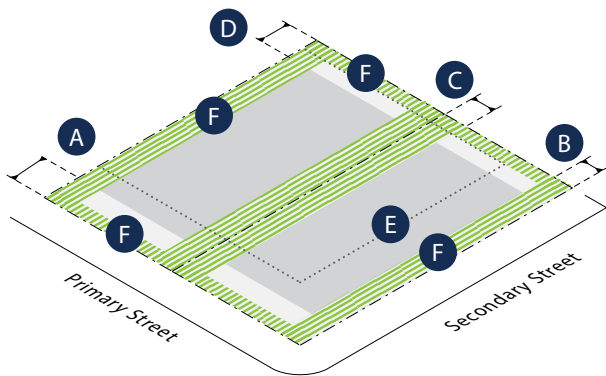
## 5.3 DETACHED BUILDING TYPES

### 5.3.1 DETACHED - D1A

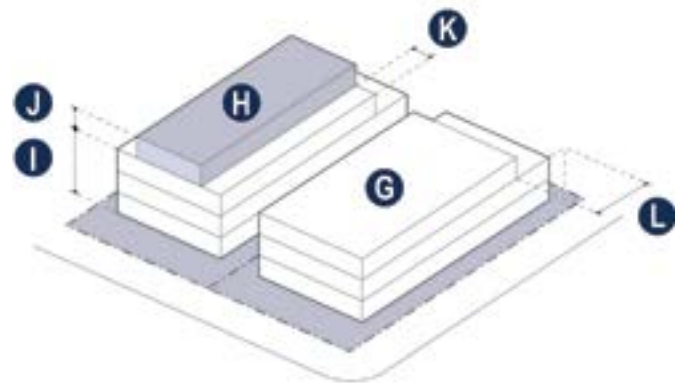
#### A. D1A BUILDING PLACEMENT

D1A types will be located in residential zones only. Apartment buildings will be separated by landscaping between lots to maintain the garden suburb feeling. Generous front setbacks are to enable ground floor courtyards and space for trees to achieve canopy coverage. Windows will face the street. Rear setbacks are to allow for trees and for rear facing apartments to have a landscaped outlook or an outlook onto rights of ways. Amalgamation of lots to create larger sites is encouraged to maximise development potential. Base building height is limited to 3 storeys and maximum heights with a bonus are limited at 4 storeys.

#### B. D1A - BUILDING SITING



#### C. D1A - BUILDING ENVELOPE



Boundary Setbacks (min)		
<b>A</b>	Primary Street Setback	6m
<b>B</b>	Secondary Street Setback	3m
<b>C</b>	Side Setback	3m
<b>D</b>	Rear Setback, to ROW	3m
	Rear Setback, to adjoining lot	4m
Parking Setbacks (min)		
<b>E</b>	Street Setback	8m
	Rear Setback	3m
Landscaping Zones, Depth (min)		
<b>F</b>	Rear, from ROW	3m
	Rear, from adjoining lot	4m

Plot Ratio (max)		
<b>G</b>	Lots less than 1000m <sup>2</sup> and/or less than 24m frontage	1.0
<b>H</b>	Lots greater than 1000m <sup>2</sup> and greater than 24m frontage	1.3
Building Height, Storeys (max)		
<b>I</b>	Lots less than 1000m <sup>2</sup> and/or less than 24m frontage	3
<b>J</b>	Lots greater than 1000m <sup>2</sup> and greater than 24m frontage	4
Upper Setbacks (min)		
<b>K</b>	Side, above 3 storeys	6m
<b>L</b>	Rear, above 2 storeys	9m

# 5.0 BUILDING TYPES

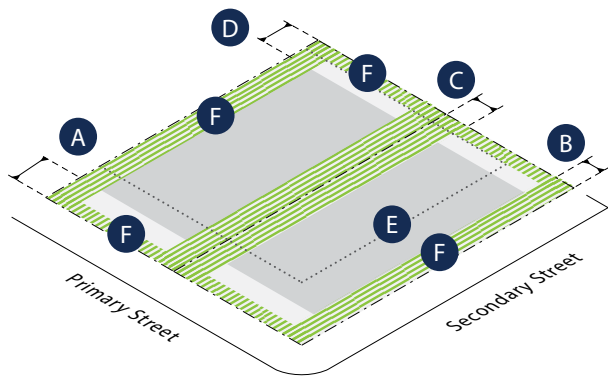
## 5.3 DETACHED BUILDING TYPES

### 5.3.1 DETACHED - D1B

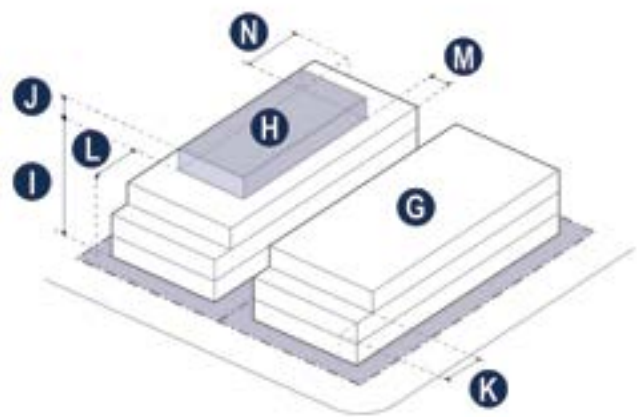
#### A. D1B BUILDING PLACEMENT

D1B types will be located in residential zones only. Apartment buildings will be separated by landscaping between lots to maintain the garden suburb feeling. Generous front setbacks are to enable ground floor courtyards and space for trees to achieve canopy coverage. Windows will face the street. Rear setbacks are to allow for trees and for rear facing apartments to have an landscaped outlook. Amalgamation of lots to create larger sites is encouraged to maximise development potential. Base building height is limited to 3 storeys and maximum heights with a bonus are limited at 4 storeys.

#### B. D1B - BUILDING SITING



#### C. D1B - BUILDING ENVELOPE



Boundary Setbacks (min)		
<b>A</b>	Primary Street Setback	3m
<b>B</b>	Secondary Street Setback	3m
<b>C</b>	Side Setback	3m
<b>D</b>	Rear Setback, to ROW	3m
Parking Setbacks (min)		
<b>E</b>	Street Setback	8m
	Rear Setback	3m
Landscaping Zones, Depth (min)		
<b>F</b>	Rear	3m

Plot Ratio (max)		
<b>G</b>	Lots less than 1000m <sup>2</sup> and/or less than 24m frontage	1.0
<b>H</b>	Lots greater than 1000m <sup>2</sup> and greater than 24m frontage	1.3
Building Height, Storeys (max)		
<b>I</b>	Lots less than 1000m <sup>2</sup> and/or less than 24m frontage	3
<b>J</b>	Lots greater than 1000m <sup>2</sup> and greater than 24m frontage	4
Upper Setbacks (min)		
<b>K</b>	Street, above 2 storeys	6m
<b>L</b>	Street, above 3 storeys	12m
<b>M</b>	Side, above 3 storeys	6m
<b>N</b>	Rear, above 3 storeys	9m



