

TUART HILL LOCAL CENTRE LOCAL DEVELOPMENT PLAN

INTRODUCTION

Where there is an inconsistency between the provisions of this Local Development Plan and other Policies the provisions of this *Tuart Hill Local Centre Local Development Plan* shall prevail. This Local Development Plan has been prepared in accordance with clause 6A.14 of Local Planning Scheme No. 3.

Objectives

- To create vibrant and active mixed use centres by locating facilities such as housing, employment places and retail activities together;
- To create a high level of pedestrian amenity and safety through the provision of continuous active retail streetscapes, controlled crossing points on Wanneroo Road, and weather shelter;
- To ensure the provision of new access ways and parking arrangements to reduce vehicular/pedestrian conflict and maximise developable areas;
- To promote a high quality built form that creates a distinctive urban form and enables safety and security through passive surveillance;
- To create public and private spaces that are safe, attractive and surrounded by active vibrant uses that will become the focal / meeting point of the centre; and
- To setback buildings on Wanneroo Road to enable the future ceding of this land for on-street parking, transit lanes, cycle lanes and wider footpaths.

Local Development Plan Area

The area subject to the Local Development Plan is illustrated in **Figure 1**.

Relationship to the R-Codes

As permitted under Clause 7.3.1 of the Residential Design Codes, this Local Development Plan contains provisions that amend or replace the following deemed to comply provisions set out in Part 6 of the R-Codes.

- Clause 6.1.3 - Street Setback
- Clause 6.1.4 - Lot Boundary Setback
- Clause 6.1.2 - Building Height

Applications Subject of this Policy

All development within the *Tuart Hill Local Centre Local Development Plan* (refer **Figure 2**) is subject to this Local Development Plan.

Variations

- Applications seeking to vary this Local Development Plan will be determined in accordance with the relevant objectives.
- The parking rates in *Local Planning Policy 6.7 Parking and Access* will not apply to the area subject to the *Tuart Hill Local Centre Local Development Plan*.

Definitions

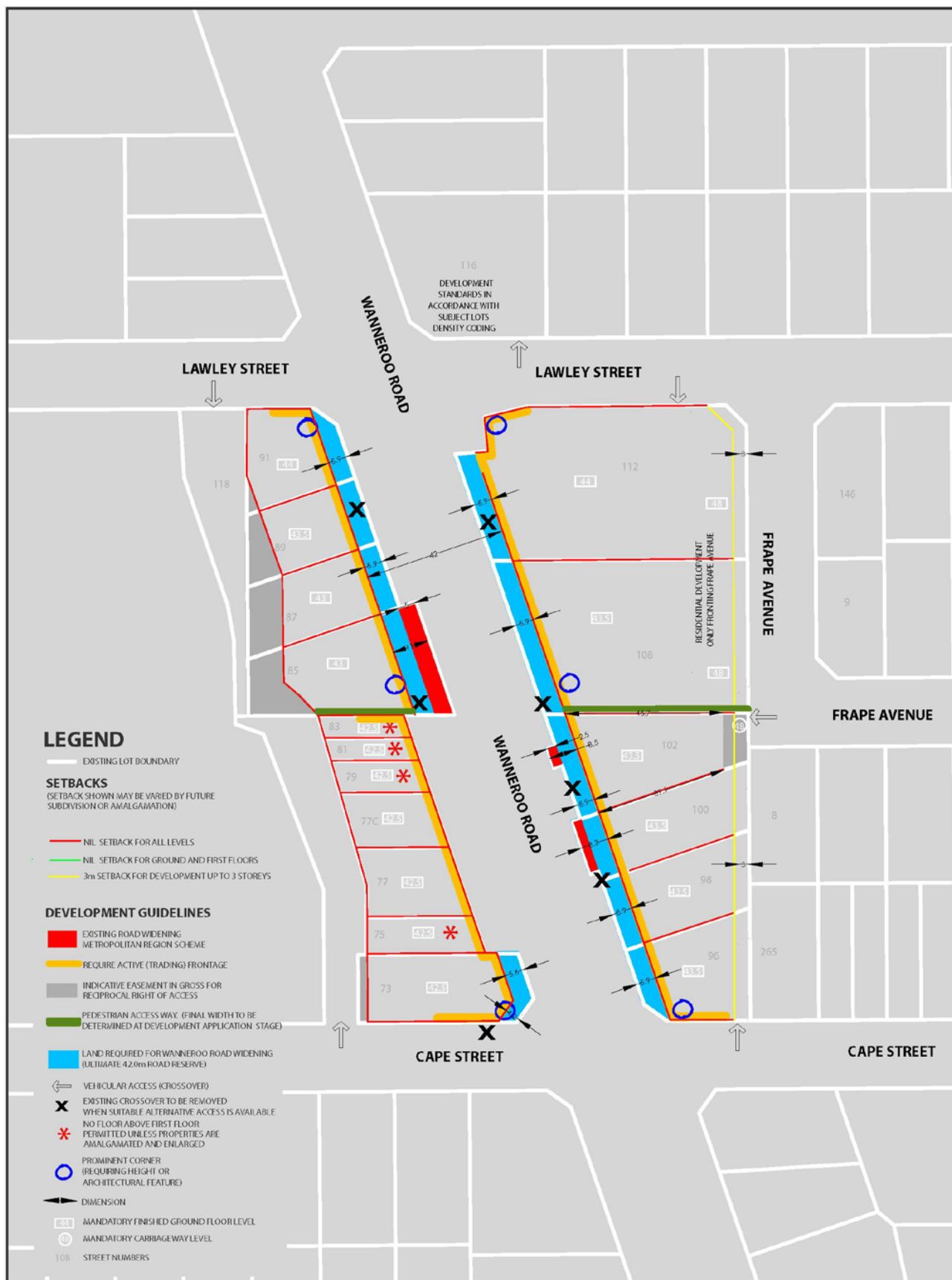
Definitions in this document shall be as per the Residential Design Codes or the City of Stirling Local Planning Scheme No. 3.

Associated Documents

The Local Development Plan shall have due regard to the *Tuart Hill Local Commercial Centre Improvement Urban Design Strategy* when assessing development applications within the policy area.



Figure 1: Tuart Hill Local Centre



DESIGN GUIDELINES

Summary Table

	Maximum Plot Ratio	Minimum Open Space (% of site)	Minimum primary street boundary setback (m)	Secondary street setback (m)	Maximum height				Maximum height of walls built up to boundary (m)	
					Storeys	Top of external wall (m)	Top of external wall (concealed roof) (m)	Top of pitched roof (m)	Maximum height (m)	Average
Wanneroo Road Frontage	N/A	N/A	Nil	Nil	6	27	28	30	14	12
Frape Ave Frontage	N/A	N/A	3m	3m	2	12	13	15	7	6

Built Form & Design

Heights

Objective

To ensure that building height and scale is appropriate to its site and context.

Provision

- The maximum height for all buildings fronting Wanneroo Road shall be six (6) storeys; and
- The maximum height of residential development fronting Frape Avenue shall be Two (2) storeys.

Residential Density Coding

Objective

To ensure that there is sufficient flexibility to achieve mixed use buildings with a residential component within the Tuart Hill Local Centre.

Provision

- Except as otherwise required in this policy, the residential component of development within the Local Development Plan area is to comply with the requirements of multiple dwellings under the RAC-0 Code.

Setbacks

Objectives

- *To strengthen the continuity of the streetscapes and provide continual weather protection for pedestrians.*
- *To ensure sufficient land for widening of Wanneroo Road to accommodate transit lanes, bike lanes, on-street parking and wider footpaths.*

Provisions

- **Street Setbacks**
 - Buildings shall be set back in accordance with Figure 2 – Local Development Plan; and
 - An additional setback of up to 3.0 metres to Wanneroo Road may be approved where necessary to provide a forecourt, building articulation, alfresco dining.
- **Side and Rear Setbacks**
 - Buildings shall be set back in accordance with Figure 2 - Local Development Plan; and
 - Minimum side setback is nil for the ground and first floor for non-residential buildings.

Communal Open Space

Objective

To ensure adequate Communal open space is provided for residents of dwellings within the Centre and ensuring green space is provided while limiting the bulk of buildings.

Provision

- The minimum communal open space for residential development is 10% of site area.

Corner Sites

Objective

To ensure that buildings give additional prominence to the street corner by using landmark features.

Provision

- On corner sites, an extra storey is allowable for a total of 25% of the Wanneroo Road and side street frontage to give visual prominence to a corner.
- Buildings located on corner sites shall include:

- Architectural roof features that protrude above the normal roof line; and
- Increased parapet heights with additional detail, colour and textures.

Facades

Objective

To ensure that building facades are architecturally interesting.

Provision

- Minimum 80% glazing on any ground floor façade. Glazing percentages apply from between 0.5m and 2.1m above the adjacent footpath/pavement level; and
- The first floor and subsequent floors above shall be articulated to break-up straight plain facades through the use of at least four of the following:
 - Openings;
 - Protruding balconies;
 - Awnings over windows;
 - Use of different colours and textures; and
 - Indentations and extrusions with details to break the building into individual elements.

Subdivision

Objective

To ensure that subdivision does not result in the requirement for vehicle access to Wanneroo Road.

Provision

- Subdivisions that require vehicle access to Wanneroo Road shall not be permitted.

Parapets

Objective

To ensure that parapets are visually interesting and to avoid flat monotonous lines.

Provision

- New buildings that contain parapets shall include:

- Indentations;
- Additional modulation; and
- Variation in parapet heights and designs so as to provide additional interest to the street.

Balconies

Objective

To ensure that the bulk of buildings is reduced.

Provision

- Balcony balustrades shall be 75% visually permeable.

Streetscape Relationship

Ground Floor Frontage

Objective

To ensure that tenancies facing the street portray an attractive and inviting frontage.

Provision

- Being predominantly clear glazed with a mixture of openings, display windows and shopfronts that allows passive surveillance of the street and the tenancies;
- Minimising the amount of signage on individual windows to no more than 20%;
- To assist in defining the street edge and to aid orientation for partially sighted pedestrians, all windows shall have a sill no less than 0.5 metres high.

Entry Points

Objective

To ensure entrances are designed to enable safe and comfortable access and that building entrances are clearly defined.

Provision

- Entry points shall directly face the street and include at least two of the following:
 - Signage above the entry door;
 - Indentation of the entry point, where recessed entrances are provided, they should be truncated at an angle to the pedestrian route of no less than 60 degrees;
 - Highlighting the entry point through the use of different materials; and

- Increasing the height of the awning above the entry point to no higher than 4.0m above footpath level.

Activity & Uses

Objective

To create an active, vibrant and safe local centre by reinforcing the interface between internal and external uses along the street front, by providing passive surveillance and the introduction of residential land uses.

Provision

- Development fronting Wanneroo Road and the intersections of Wanneroo Road and Cape Street and Lawley Street shall have active (trading) frontages in accordance with Figure 2;
- Active and lively street fronts shall be provided that encourage pedestrian activity and vitality of the centre by:
 - Locating retail and other active retail uses on the ground floor level; and
 - Locating office and other non-active uses (residential) on upper levels.
- If it is proposed to undertake a complete redevelopment of a property (e.g. substantially demolish existing building and replace with a new building) a minimum of 20% of the Gross Floor Area of the new building shall be for residential purposes.

Weather Protection

Objective

To support a comfortable external environment for pedestrians.

Provision

- Awnings shall be provided over all footpaths that abut a building, including footpaths that provide access to the rear of buildings;
- Awnings shall be provided above all entrances and exits of a building;
- New awnings shall line up with existing awnings (where present);
- New awnings shall protrude from the face of the building by a minimum width of 2.0m (where possible); and
- Awnings shall be parallel to the footpath

Levels

Objective

To ensure that development follows the topography of the land and to ensure that there is equitable access for pedestrians and that excessive differences will not have a negative impact on the amenity of the area.

Provision

- The finished floor levels of any new or redeveloped buildings shall be in accordance with the levels annotated on **Figure 2**.
- The reciprocal laneway on No 102 Wanneroo Road shall be in accordance with the levels annotated on **Figure 2**.
- On sloping sites new developments shall be stepped so as to avoid large differences between the footpath level and the finished level of the shop front; and
- There shall be no difference between the ground floor and the footpath level of a building to ensure pedestrian access, for non-residential uses; and
- The finished floor level of residential uses may be elevated a maximum of 1.2m above footpath level.

Fencing & Gates

Objective

To provide an open, accessible and attractive urban environment.

Provision

- Fencing between the building and the street boundary shall not be permitted for non-residential buildings.
- Fencing may be permitted for alfresco areas where buildings are set back from the street boundary and the fence is permeable with a maximum height of 1.2 metres;
- Fencing behind the building line shall generally not be permitted where it obstructs access to public parking areas;
- Where required, gates & fences shall be open style to 1.8m;
- No barbed wire or electric fencing shall be permitted; and
- Fencing on Frape Avenue is to be in accordance with the R-Codes.

Landscaping

Objective

To improve the visual appeal of development, screen service areas and provide shade and green relief in built up areas.

Provision

- A landscaping plan shall be submitted for all new development applications and be in accordance with the City's Landscaping Policy; and
- Planters and window boxes are acceptable where the footpath is sufficiently wide enough to accommodate such features

Parking

Parking Rates

Objective

To facilitate the development of adequate parking facilities that does not have a detrimental impact on character and amenity of the centre and encourages alternate forms of transport.

Provision

All parking is to be in accordance with the following rates:

- Shop (1,000m² or greater in area): 5.0 bays/100m² of Net Leasable Area
- Other Non-Residential Uses: 3.0 bays/100m² of Net Leasable Area
- Residential – in accordance with Residential Design Codes of Western Australia.

The reduction in parking allowed under in Local Planning Scheme No.3, *Local Planning Policy 6.7 Parking and Access*, shall not apply to the area subject to the *Tuart Hill Local Centre Local Development Plan*.

On-Street Parking

Objective

To ensure sufficient on street parking is provided on secondary streets within the centre.

Provision

- If it is proposed to undertake a complete redevelopment of a property with a secondary street frontage (e.g. demolish existing building and replace with a new building) on street parking shall be provided on all secondary streets to the satisfaction of the City.
- For every one on-street bay provided the required number of onsite parking bays may be reduced by 0.5 bays.

Multi Storey Car Parks

Objective

To ensure that multi storey car parks are not visible from the street and do not have a detrimental impact on the streetscape.

Provision

- Standalone Multi Storey Car Parks shall not be visible from the street and shall be located behind buildings in accordance with Figure 3; and
- Multi Storey Car Parks included within the main building shall be screened / treated to the rear boundary so as to provide a seamless appearance between the car park levels and other levels.

Design & Location of Car Parking Spaces

Objective

To ensure that car parking areas do not disrupt the continuity of commercial frontages or otherwise detract from the amenity of the streetscape.

Provision

- Parking areas shall not be visible from the street and located behind the building line as identified in **Figure 3**; and
- Parking bays shall be designed in accordance with the relevant Australian Standard and shall have 1 tree per 6 parking bays, as required by the City's Landscaping Policy.

Bicycle Parking Facilities

Objective

To encourage the use of bicycles for all types of journeys.

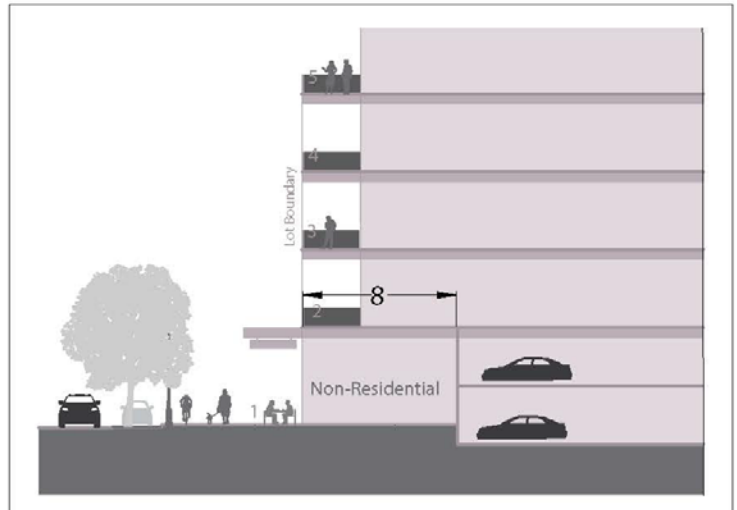
Provision

- The provision of bicycle parking facilities shall be in accordance with the City's Bicycle Parking Policy.

MIXED USE SITES

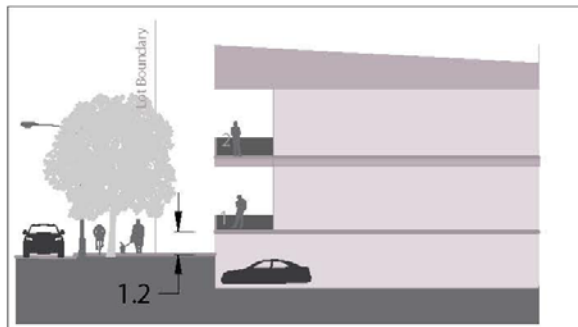
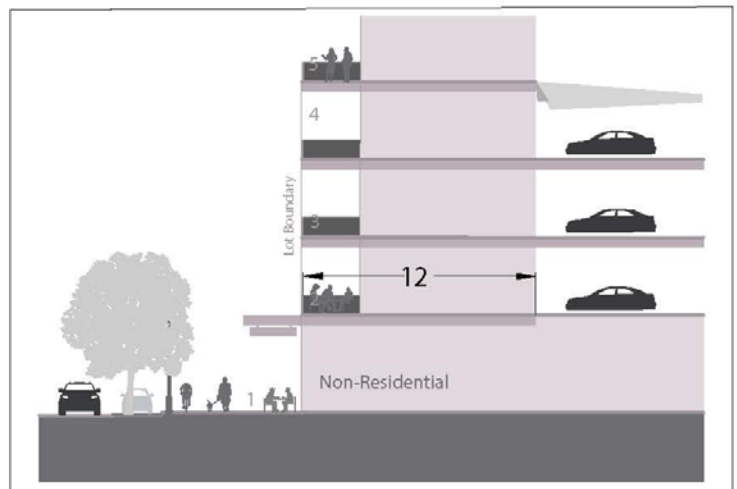
Ground Floor Parking

Multi Storey Carparks sleeved 8 meters behind active ground floor use



Above Ground Parking

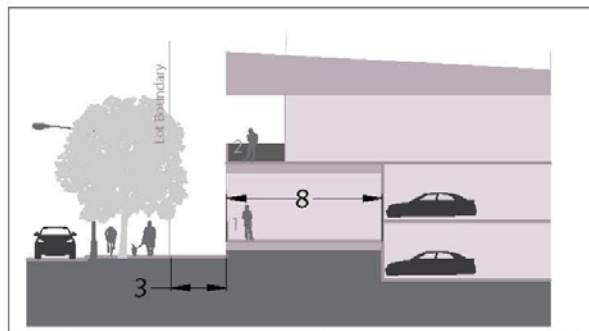
Above ground parking 12m sleeve behind upper-floor uses and shaded.



RESIDENTIAL SITES

Basement Parking to Frape Avenue

Basement parking with a nil setback to street boundary (appropriate screening shall be provided to car parks)



Ground Floor Parking to Frape Avenue

Multi Storey Carparks sleeved 8 meters behind ground floor.

FIGURE 3: LOCATION OF PARKING AREAS

Access

Vehicle Access

Objective

To ensure that vehicle access ways are safe and easily traversed.

Provision

- Any Approval to Commence Development granted to undertake a complete redevelopment of No's 73, 85, 87, 89, 91 and 102 Wanneroo Road (e.g. substantially demolish existing building and replace with a new building) shall include a condition requiring the establishment of a reciprocal rights access easement in gross for the purposes of providing access over the subject property in accordance with **Figure 2**. All landholdings and the City shall be party to the easement (where possible);
- Vehicle access to sites fronting major roads shall be from side streets or rights of ways where available;
- All vehicle movements shall be able to enter and exit the site in a forward gear; and
- All parking areas to comply with the Australian Standard 2890.1 in relation to turning circles, gradients within the site.

Pedestrian Access

Objective

To create a pedestrian network that is safe and direct.

Provision

- If it is proposed to undertake a complete redevelopment of a property (e.g. substantially demolish existing building and replace with a new building) a condition of Approval to Commence Development for 85 and 108 Wanneroo Road shall be the creation of a Pedestrian Access Way easement, as annotated on Figure 2. The final location width and height of the Pedestrian Access Way will be determined at the development application stage; however it shall not exceed six (6) metres in width. If a building is constructed over the pedestrian access, the pedestrian access shall be no less than three (3) metres in height;
- Pedestrian access, in the form of a footpath, shall be provided from the parking area to the entry point of the proposed development and along all street frontages;
- For developments with parking at the rear, pedestrian access between the street and car parking area shall be provided;
- Pedestrian routes shall as far as possible be on publicly owned land, and preferably be within the road reserve as part of the street network;
- Pedestrian routes shall be aligned primarily along existing building fronts and thereafter along proposed new development fronts;

- Pedestrian routes shall be as direct and level as possible; and
- Dead ends and/or closed view corridors shall be avoided.

Crossovers

Objective

To limit the number of crossovers to reduce the impact on pedestrians and traffic.

Provision

- Crossovers shall be in accordance with **Figure 2**.
- The existing crossovers to be removed as annotated on **Figure 2** shall be closed as a condition of Approval to Commence Development on the affected lots, subject to suitable alternative access being available, unless otherwise agreed with Main Roads WA and the City of Stirling.
- All crossovers shall be a maximum width of 6.0m.

Service Access and Facilities

Objective

To ensure there is sufficient space for the storage of waste and other materials and sufficient space for the delivery of goods.

Provision

- The provision of service access to all commercial premises shall be provided for loading and unloading goods.
- Bin storage areas shall be provided in accordance with the City's Bin Storage Areas Policy.

Lighting, Safety & Security

Lighting

Objective

To ensure that developments support proper and attractive illumination of public and private spaces for security and safety.

Provision

- Lighting shall be provided in the following areas to increase safety and security:
 - Under all awnings;
 - In all parking areas;
 - Service areas;
 - Of all footpaths;
 - Of all entry points; and
 - Additional lighting of key elements and features of the building and landscaping is encouraged to add vitality.

Closed Circuit Television (CCTV)

Objective

To provide for the future installation of CCTV for all Night Clubs, Hotels and Taverns.

Provision

- All Night Clubs, Hotels and Taverns shall provide Closed Circuit Television Cameras;
- Cameras shall be placed as to ensure that all entries are covered by cameras;
- Locations shall be cognisant of night lighting levels with additional lighting provided if necessary; and
- Any CCTV infrastructure sharing between the City and property owners shall be subject to legislative requirements and management agreements.

Safety & Surveillance

Objective

To ensure that public and private areas are either visible and safe or screened and illuminated in such a way as to ensure a high quality safe and comfortable outdoor environment prevails.

Provision

- The following design features shall be avoided to improve safety and reduce illegal graffiti:
 - Entrapment areas, blind corners and narrow pathways;
 - Long expanses of blank walls (treatment with anti-graffiti paint required where permitted);
 - Dead ends and hidden recesses shall be avoided;
 - Landscaping and other elements shall not create a visual barrier between 0.5 and 2.0m above finished floor levels or ground level as applicable;
 - Rear loading shall be secure at night and enclosed to reduce light and noise spill during night loading;
 - Loading bay access lanes and other areas that may be dead ends at night shall be secured; and
 - Rear parking and pick-up/delivery areas shall be under passive surveillance from active indoor areas.

Roller Shutter Doors

Objective

To ensure that ground floor areas provide an attractive frontage to the street and other visible spaces whilst providing security.

Provision

- Solid roller shutter doors shall not be permitted on any façade facing the street; and
- Roller doors of see-through acrylic material are acceptable on shop fronts providing that at least 75% of the roller door is transparent and the material maintains a high level of transparency once installed.

Screening

Objective

To ensure that air conditioners and other services do not detract from the streetscape.

Provision

- Air conditioning units, solar panels, wind turbines, ducts and other services shall be screened from view and should be located away from the street front, or otherwise integrated into the design of the building.

Shopping Trolleys

Objective

To ensure that shopping trolleys are contained within the boundary of the local centre and do not cause any detrimental impact to the amenity of surround areas.

Provision

- All developments that utilise shopping trolleys shall prepare a Shopping Trolley Management Plan to the satisfaction of the Manager Community Safety; and
- The Shopping Trolley Management Plan is to address such issues, but not limited to, collection of trolleys from surrounding areas and trolley storage.

Other Considerations

Signage

- Advertising Signs shall be in accordance with the City's Advertising Signs Policy.

Bin Storage Area

- A refuse storage area is required for all developments in accordance with the City's Bin Storage Areas Policy.

Sound Attenuation

Objective

To ensure that noise from non-residential uses does not adversely affect the amenity of residential development.

Provision

- All mixed-use developments containing residential uses shall submit an acoustic report prepared by a qualified acoustic (noise) consultant.
- Developments shall:
 - Ensure noise sensitive areas (such as bedrooms) are located away from noise sources, where possible;
 - Use 10mm glazing or double glazing where windows face a noise source;
 - Use of appropriate materials between floors, walls, ceilings and doors to minimise noise; and
 - Endeavour to comply with the Environmental Protection (Noise) Regulations 1997.

VARIATIONS

Variations to this Local Development Plan will be assessed against the objectives of this Plan.

OFFICE USE ONLY:

Local Development Plan Adoption and Amendment

Action	Resolution Number	Effective Date
Adopted	1115/035	17 November 2017