

Address: Lot 112 (#752) Beaufort St, Mt Lawley

Childcare Centre

Job Number: 22082

City of Stirling
17 April 2026
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Drawing No	Description
01	Cover Page
02	3D Street Perspective
03	3D Perspectives
04	Existing Site Survey
05	Site Plan
06	Context Plan
07	Overlay
08	Ground Floor Plan
09	First Floor Plan
10	Elevations
11	Elevations
12	Roof Plan
13	Section & Solar Study
14	Breeze Path Plan



760 BEAUFORT STREET, MOUNT LAWLEY



746 BEAUFORT STREET, MOUNT LAWLEY

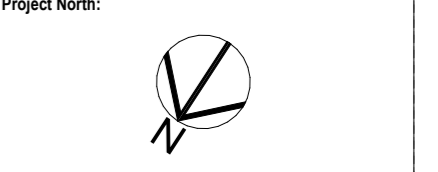


Revision	Description	Date
7.00	Development Approval	15.04.26
6.00	Development Approval	04.02.26
5.00	Heritage Comments	17.10.25
4.00	Heritage Comments	02.08.25
3.00	DRP	10.04.25
2.00	Changes	04.04.25
1.00	Concept	26.02.25

Project Name:
Childcare Centre
Project Address:
Lot 112 (#752) Beaufort St, Mt Lawley
Drawing Title:
3D Street Perspective
Scale:
1:3.33
Project No:
22082
Drawing No.:
02 of 14



Revision	Description	Date
7.00	Development Approval	15.04.26
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2.00	Changes	04.04.25
1.00	Concept	28.02.25



Project Name
Childcare Centre
Project Address
Lot 112 (#752) Beaufort St, Mt Lawley

Drawing Title:
3D Perspectives

Scale: Sheet Size: A1

Project No: 22082 Revision Number: 7.00

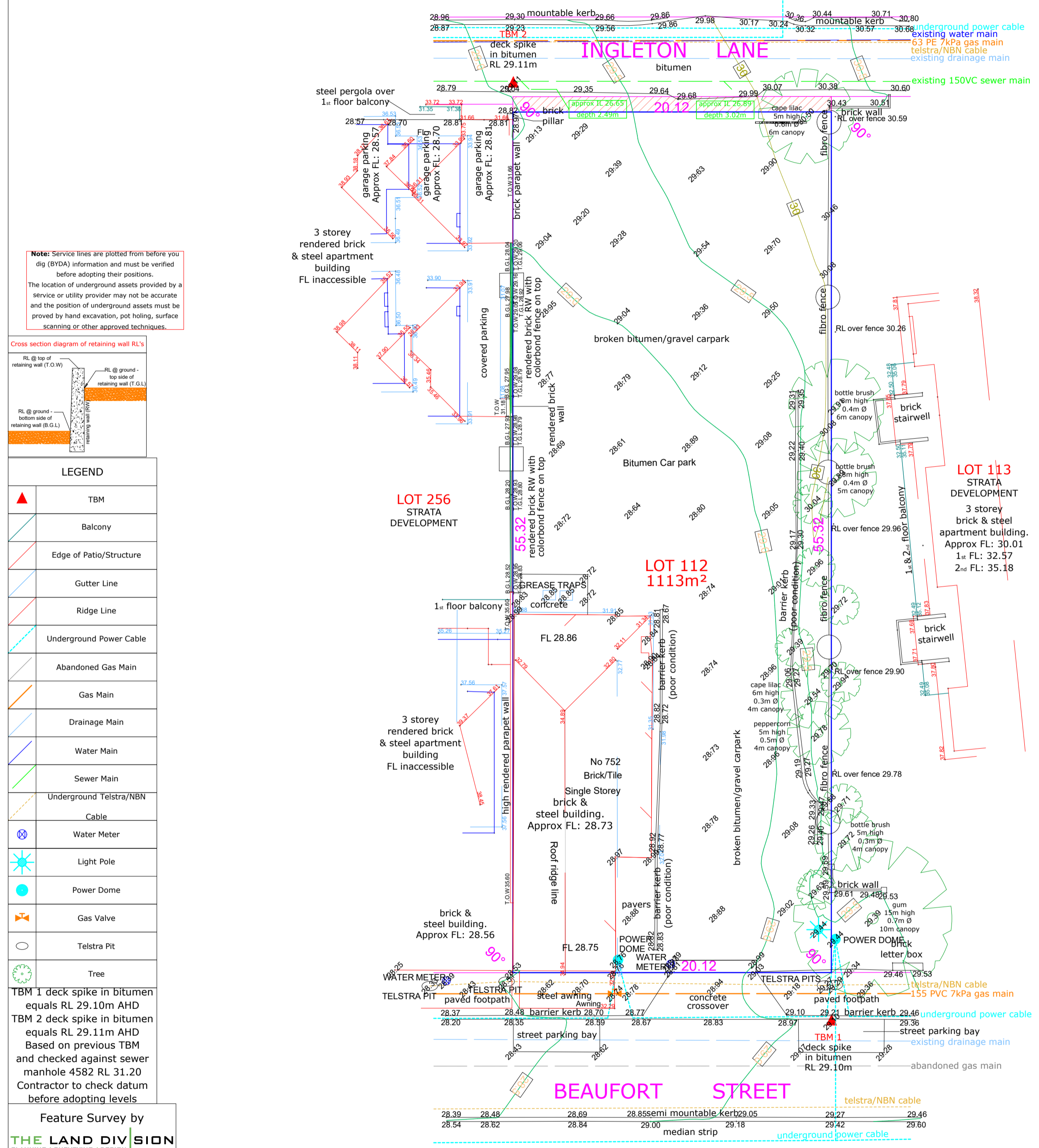
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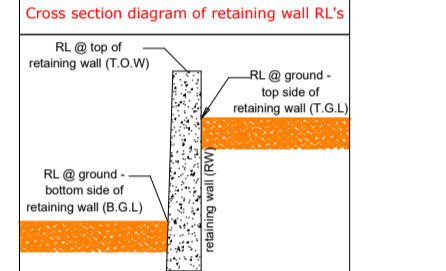
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Note: Service lines are plotted from before you dig (BYDA) information and must be verified before adopting their positions. The location of underground assets provided by a service or utility provider may not be accurate and the position of underground assets must be proved by hand excavation, pot holing, surface scanning or other approved techniques.



LEGEND	
	TBM
	Balcony
	Edge of Patio/Structure
	Gutter Line
	Ridge Line
	Underground Power Cable
	Abandoned Gas Main
	Gas Main
	Drainage Main
	Water Main
	Sewer Main
	Underground Telstra/NBN Cable
	Water Meter
	Light Pole
	Power Dome
	Gas Valve
	Telstra Pit
	Tree

TBM 1 deck spike in bitumen equals RL 29.10m AHD
TBM 2 deck spike in bitumen equals RL 29.11m AHD
Based on previous TBM and checked against sewer manhole 4582 RL 31.20
Contractor to check datum before adopting levels

Feature Survey by
THE LAND DIVISION
PLANNING & SURVEYING | DESIGN
PO Box 2444,
Malaga, WA 6090
phone: 08 9209 3232
www.landdivision.com.au

Survey Date: 22 September 2025 Scale 1:200@A3
Client: Germano Designs

Rev	Date	Description	Surv	Drawn
0	24/09/2025	Feature Survey Drafted	TF	TF

FEATURE AND CONTOUR SURVEY OF LOT 112 ON PLAN 2473
752 Beaufort Street, Mount Lawley
C/T Vol: 1485 Fol: 145
our ref. 25-0860

NOTES: 1) CONSULT LEGAL ADVICE ON EASEMENTS, ENCUMBRANCES AND CAVEATS THAT MAY APPEAR ON THE CERTIFICATE OF TITLE. 2) LEVELS ON ADJOINING PROPERTIES ARE APPROXIMATE DUE TO ACCESS RESTRICTIONS. 3) SERVICES PLOTTED AS VISUALLY SEEN ON SITE AND ARE APPROXIMATE. 4) SEWER POSITION AND LEVELS FROM WATER CORPORATION PLANS. 5) CONSULT BEFORE YOU DIG (BYDA) TO CHECK LOCATION OF UNDERGROUND SERVICES. 6) BEWARE OF OVERHEAD POWER LINE HAZARDS. 7) CONSULT TLD ON ANY ANOMALY BEFORE DESIGN AND CONSTRUCTION. 8) POSITION AND DEPTH OF SERVICES TO BE CONFIRMED ON SITE BY CONTRACTOR. 9) FEATURES ARE RELATED TO FENCE LINES ONLY. NO CONNECTION MADE TO BOUNDARIES. REPEP RECOMMENDED.



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2.00 Changes	04.04.25
1.00 Concept	28.02.25



Project Name
Childcare Centre
Project Address
Lot 112 (#752) Beaufort St, Mt Lawley
Drawing Title:
Existing Site Survey

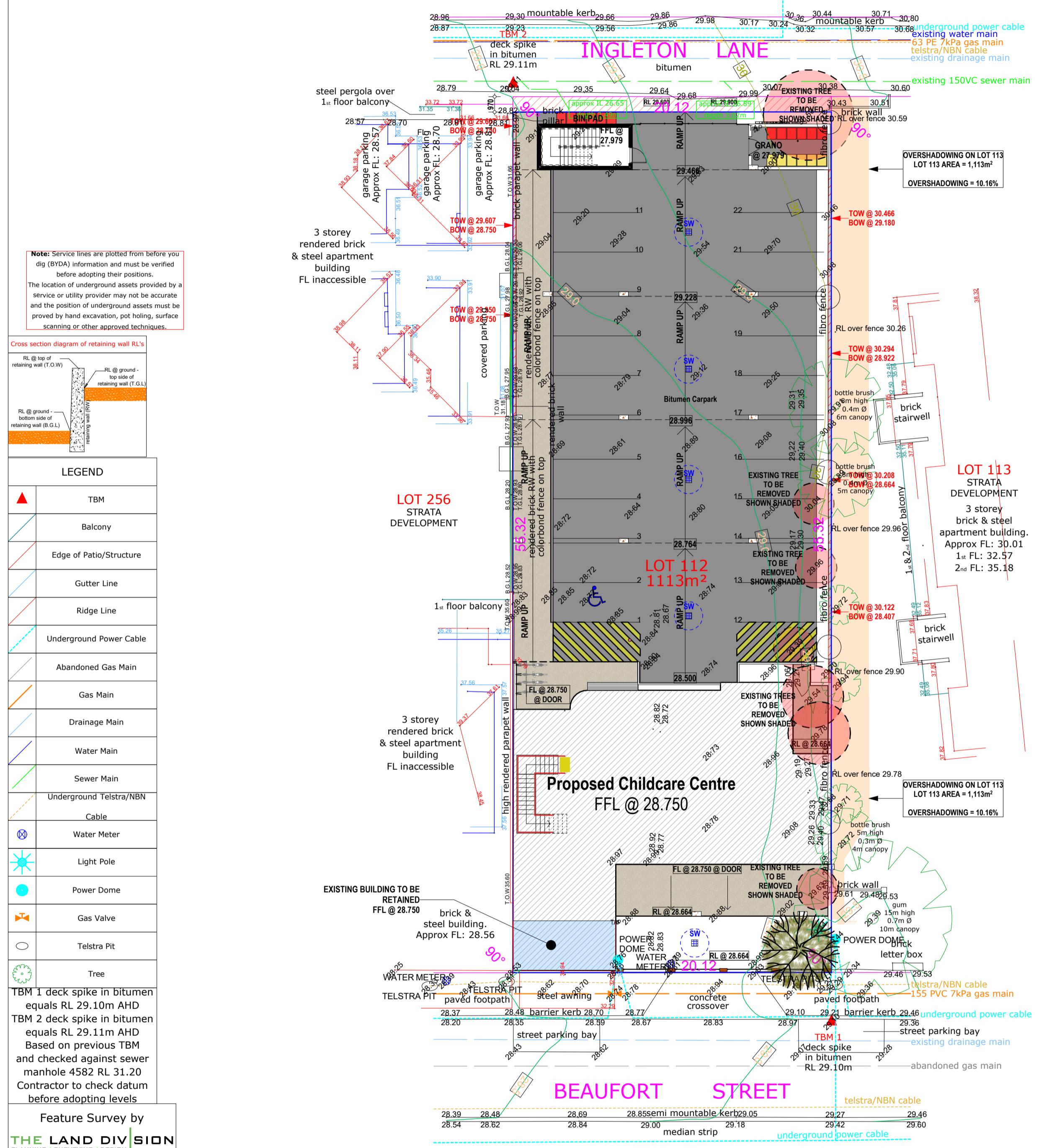
Scale:	Sheet Size:
1:200	A1
Project No:	Revision Number:
22082	7.00

Drawing No.:
04 of 14

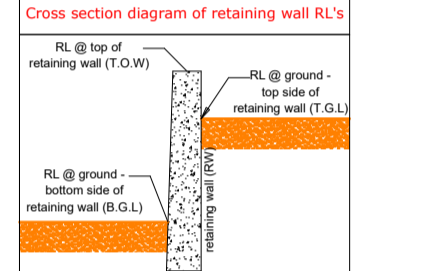
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	Underground Power Cable
	Abandoned Gas Main
	Gas Main
	Drainage Main
	Water Main
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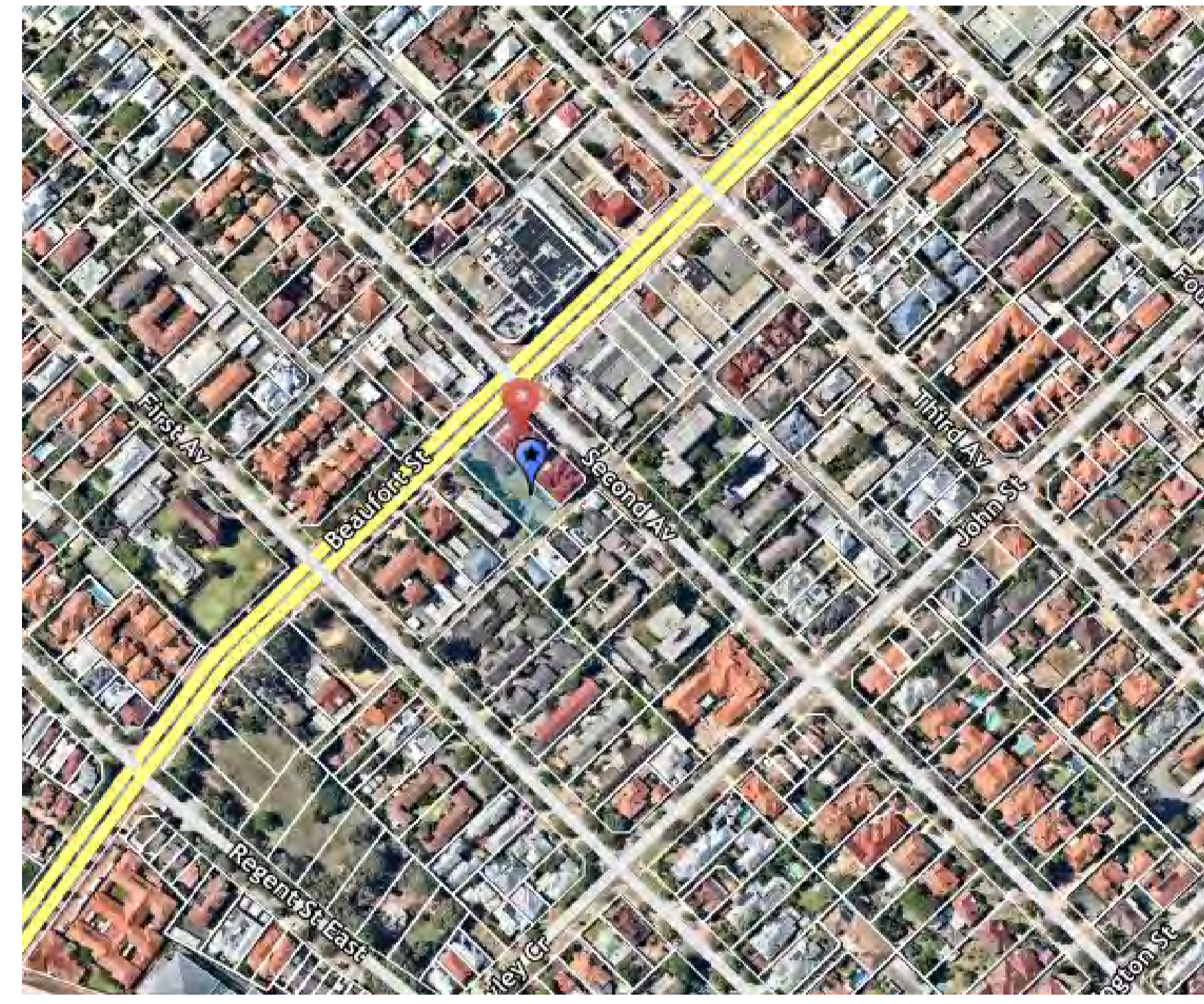
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FEATURE AND CONTOUR SURVEY OF LOT 112 ON PLAN 2473
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C/T Vol: 1485 Fol: 145
our ref. 25-0860

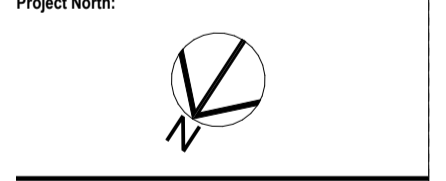
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Zone	Area	Perim
First Floor		
Fire Stairs	17.34	17.780
Chickcare	461.68	119.862
	479.02 m²	137.642 mm
Ground Floor		
Services	10.80	13.800
Fire Stairs	14.88	16.880
Fire Stairs	17.34	17.780
Chickcare	252.39	76.089
	296.41 m²	123.749 mm
	774.43 m²	261.391 mm

Site Calculations	
Site Area:	1,113m²
Building Footprint:	270.32m²
Site Coverage:	24.28%
Allowable Site Coverage:	NA%
Zoning:	NA
Practices:	Mixed Use
Heritage:	NA
Bushfire:	NA
BAL:	NA
Acoustic:	TBC
Sewer:	Ingleton Lane
Power:	Above
Coastal:	NA
Water:	TBC
Wind Rating:	TBC

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2.00	Changes	04.04.25
1.00	Concept	28.02.25



Project Name
Childcare Centre
Project Address
Lot 112 (#752) Beaufort St, Mt Lawley
Drawing Title:
Site Plan

Scale:	Sheet Size:
1:200	A1
Project No: 22082	Revision Number: 7.00

Drawing No.:
05 of 14

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66 Second Ave, Mount Lawley 6050



760 Beaufort St, Mount Lawley 6050



31 Lawley Cres, Mount Lawley 6050



755 Beaufort St, Mount Lawley 6050

Revision	Description	Date
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Project North:



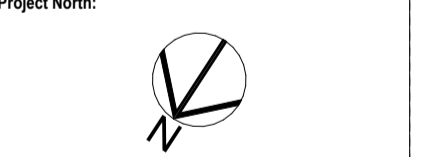
Project Name
Childcare Centre
Project Address
Lot 112 (#752) Beaufort St, Mt Lawley
Drawing Title:
Context Plan

Scale: Sheet Size: A1
Project No: 22082 Revision Number: 7.00
Drawing No.: 06 of 14





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Project Name
Childcare Centre
Project Address
Lot 112 (#752) Beaufort St, Mt Lawley
Drawing Title
Overlay

Scale: Sheet Size: A1

Project No: 22082 Revision Number: 7.00

Drawing No.: 07 of 14



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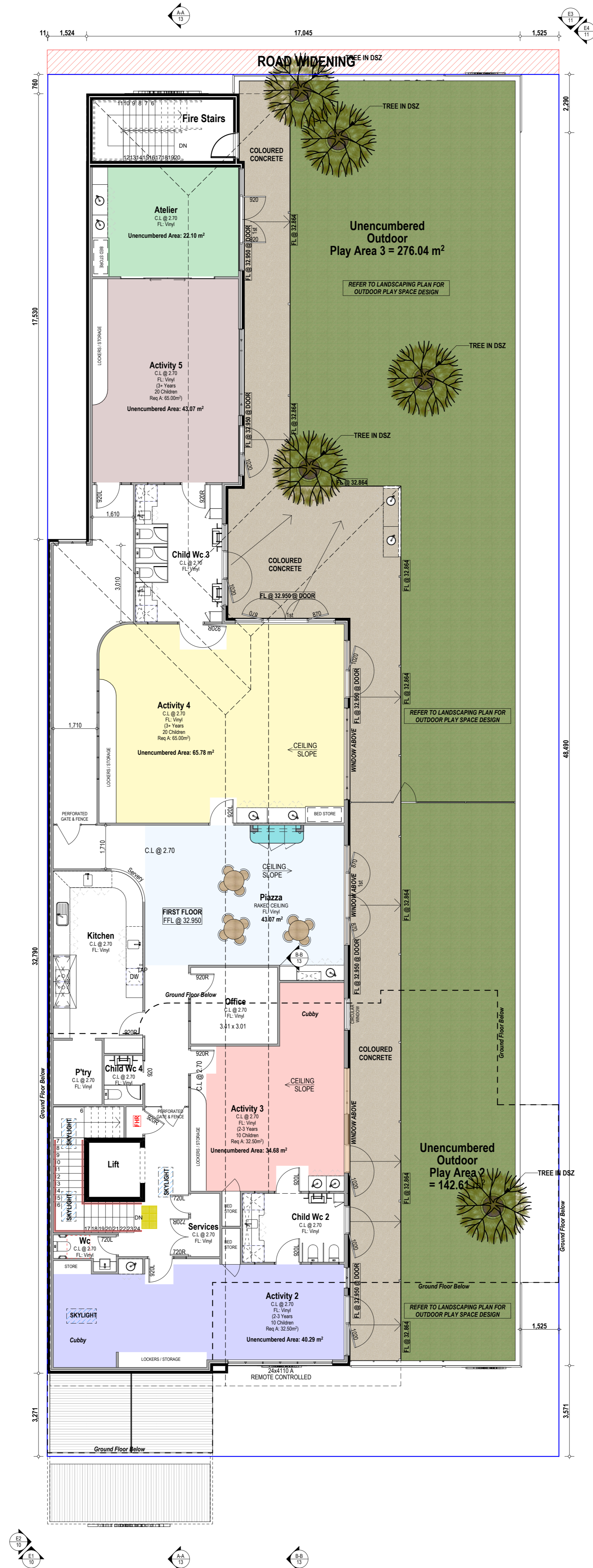
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Childcare Calculations

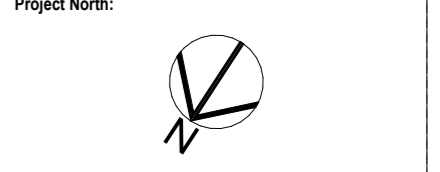
Room	Age (Yrs)	Quant.	Size	Staff Req
Activity 1	0-2	12	41.49m ²	3
Activity 2	2-3	10	40.29m ²	2
Activity 3	2-3	10	34.68m ²	2
Activity 4	3+	20	65.78m ²	2
Activity 5	3+	20	43.07m ²	2
Atelier			22.62m ²	
Piazza			43.07m ²	
				2

Total Internal = 72 **291.007m² 13**
(Min 3.25m² per child) (Min 234.00m² req)

Total External Play Area = 72 **506.88m²**
(Min 7m² per child) (Min 504.00m² req)



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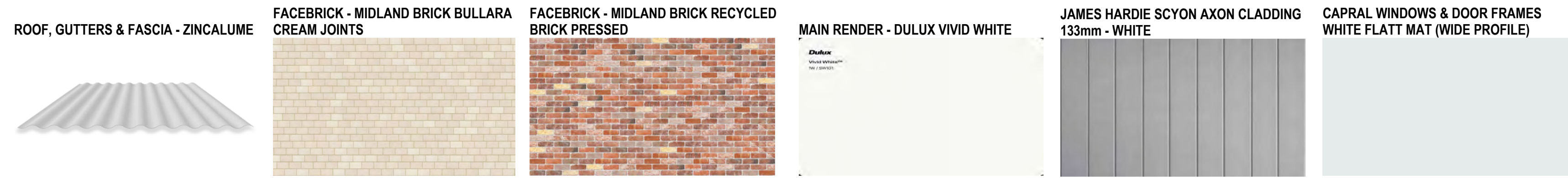


Project Name
Childcare Centre
Project Address
Lot 112 (#752) Beaufort St, Mt Lawley
Drawing Title
First Floor Plan

Scale:	Sheet Size:
1:100	A1
Project No:	Revision Number:
22082	7.00

Drawing No.:
09 of 14

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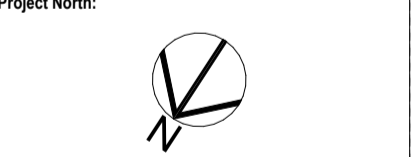


E1 North West Elevation
1:100



E2 North East Elevation
1:100

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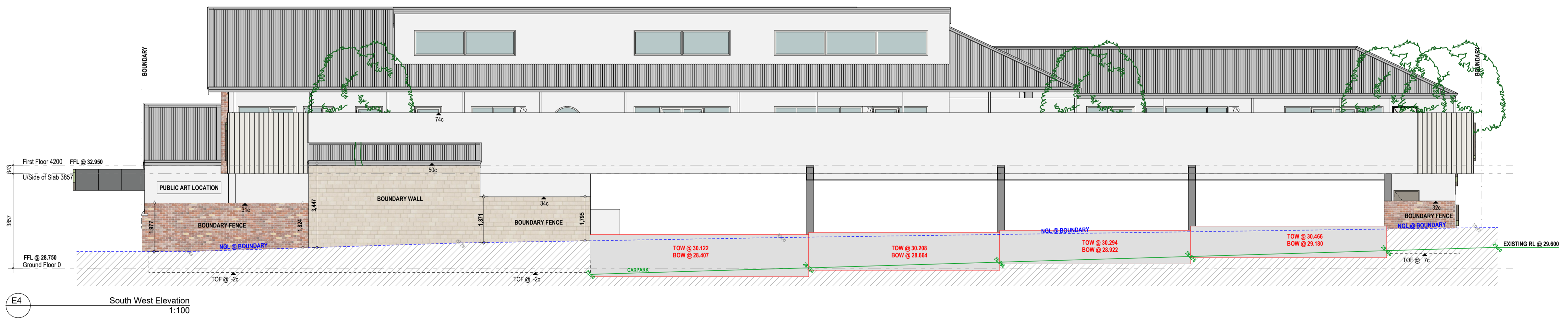
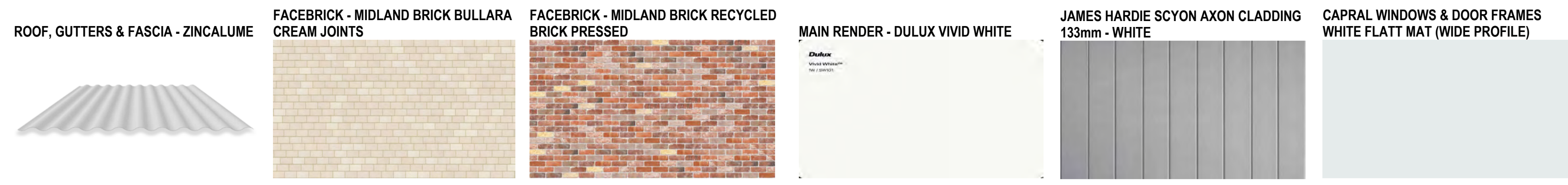
Project Name
Childcare Centre
Project Address
Lot 112 (#752) Beaufort St, Mt Lawley
Drawing Title:
Elevations

Scale: 1:100	Sheet Size: A1
Project No: 22082	Revision Number: 7.00

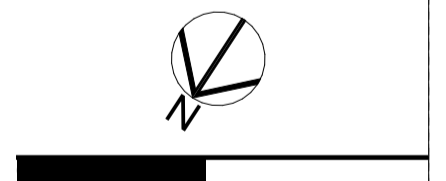
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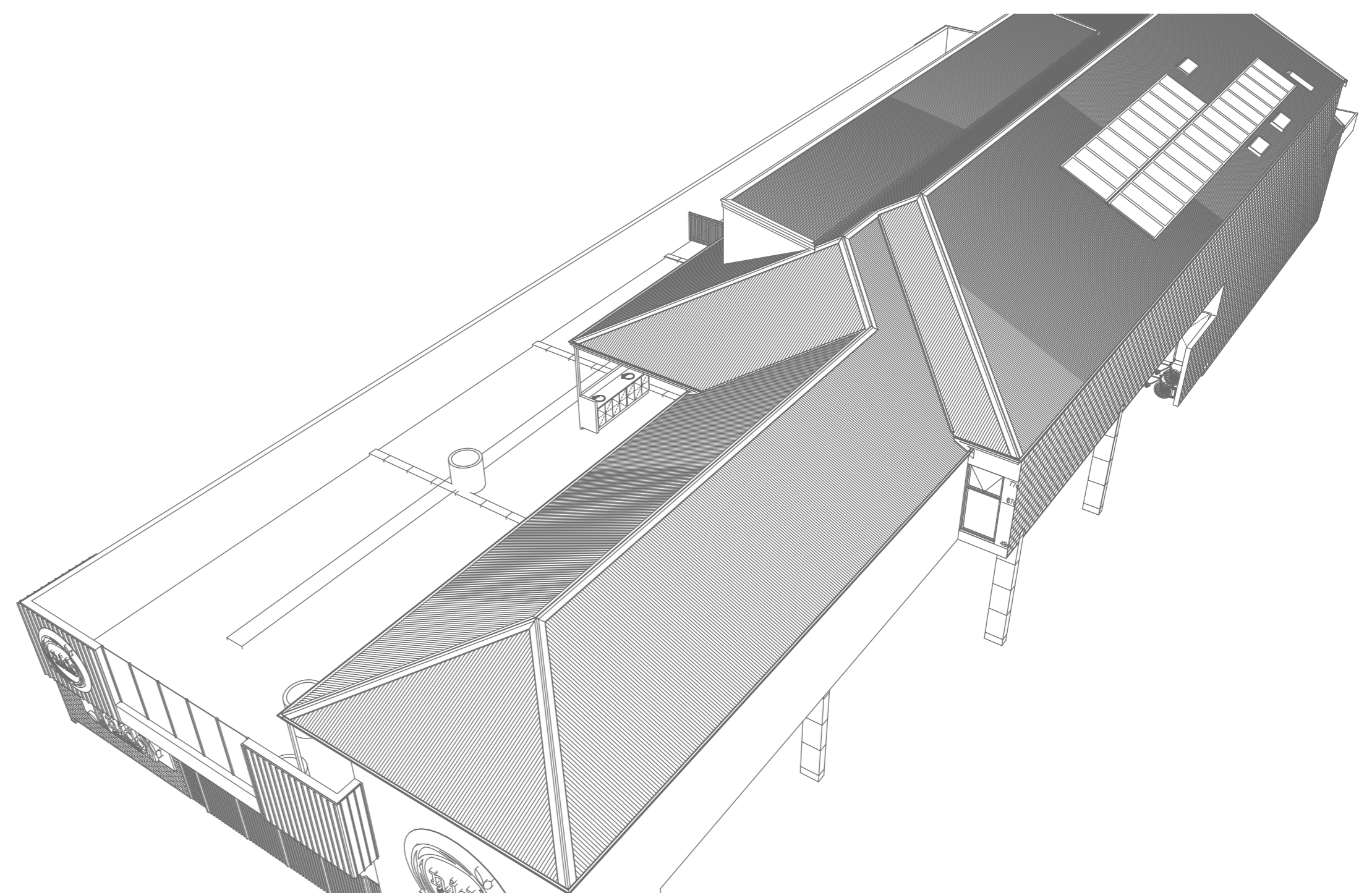
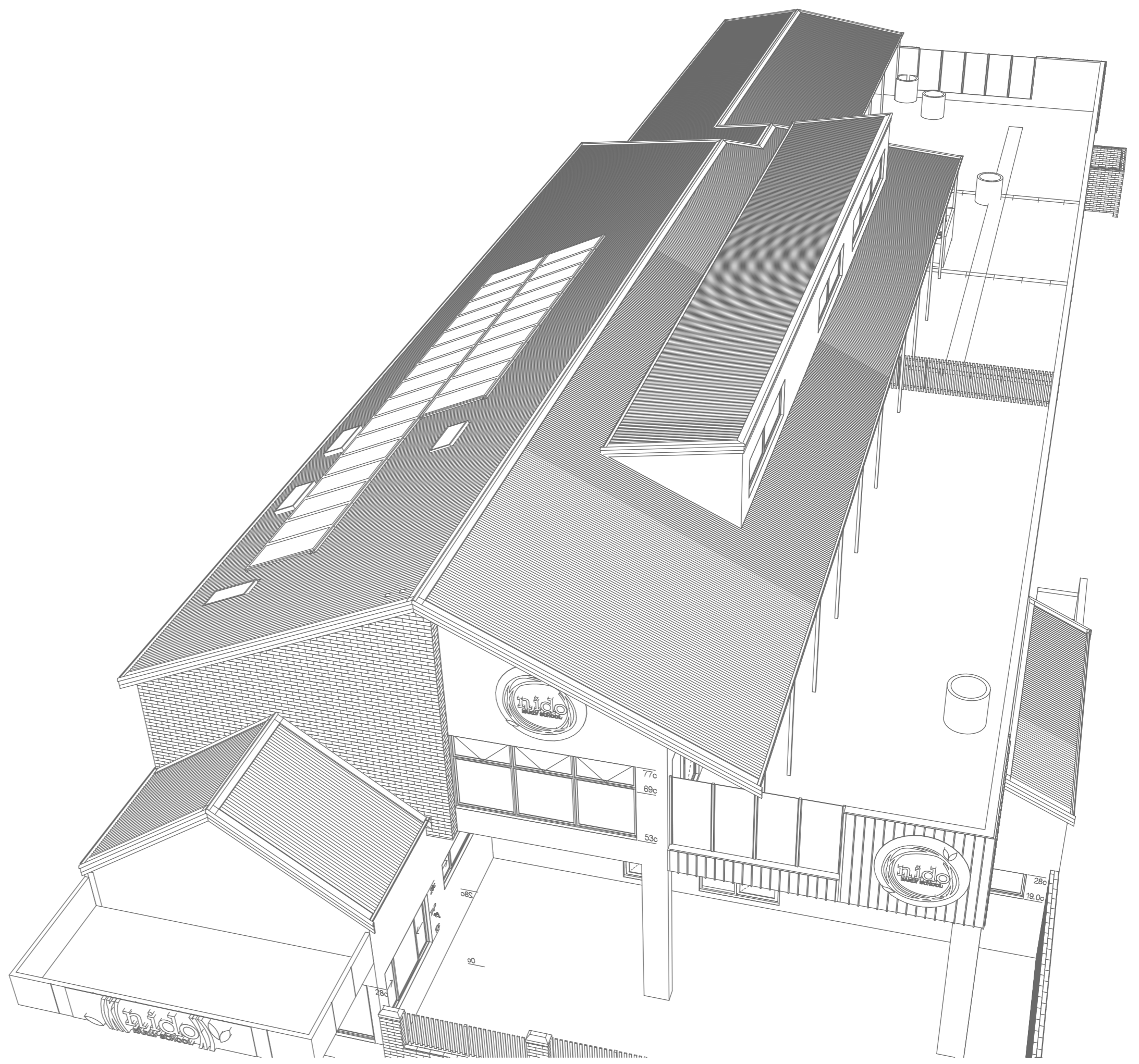
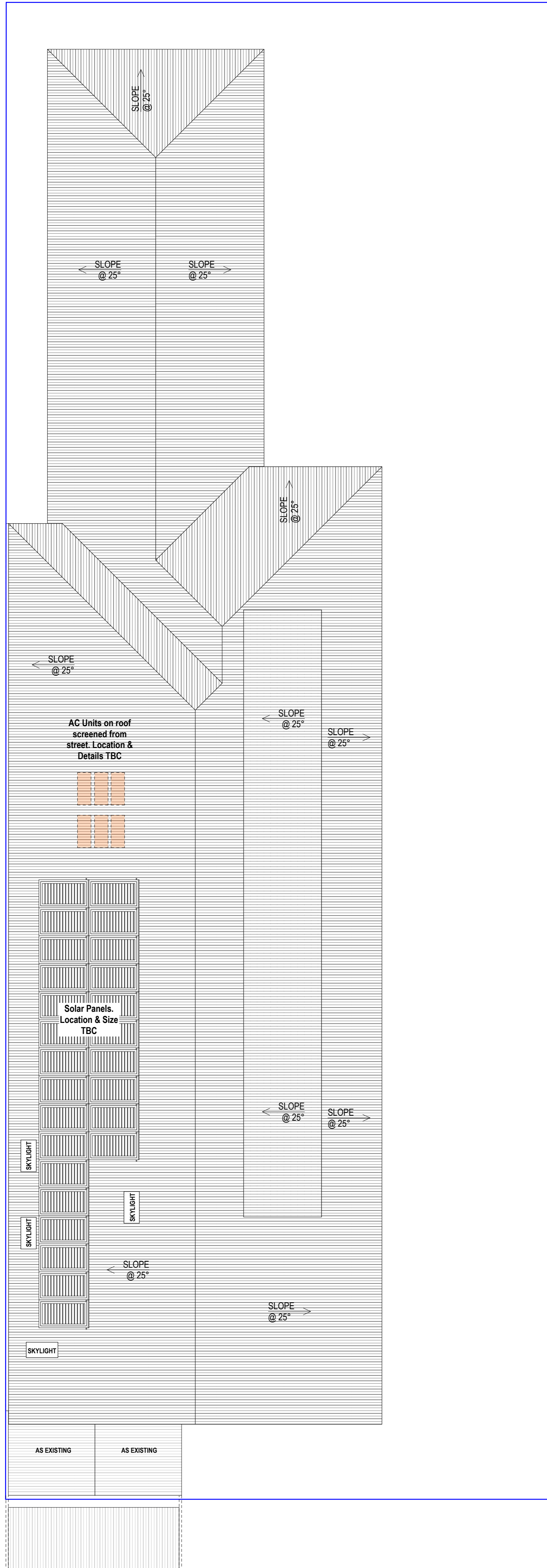
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Drawing Title:
Elevations

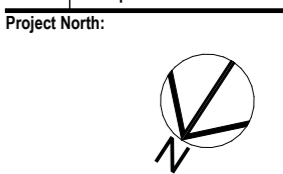
Scale:	Sheet Size:
1:100	A1
Project No:	Revision Number:
22082	7.00

Drawing No.:
11 of 14



Our proposed childcare center has been thoughtfully designed with sustainability in mind. The building will include solar panels to support renewable energy use and reduce our carbon footprint. Roof structures will provide shading for windows, helping to naturally regulate indoor temperatures and reduce reliance on air conditioning. To further conserve energy, we are installing high-quality insulation and LED lighting throughout the facility. Water conservation measures will be implemented, alongside a dedicated recycling program to minimize waste. Additionally, bike bays will be provided to encourage environmentally friendly transport and promote a healthy lifestyle. Landscaping will feature native planting on the front verge, supporting local biodiversity and reducing water usage.

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Drawing Title
Roof Plan

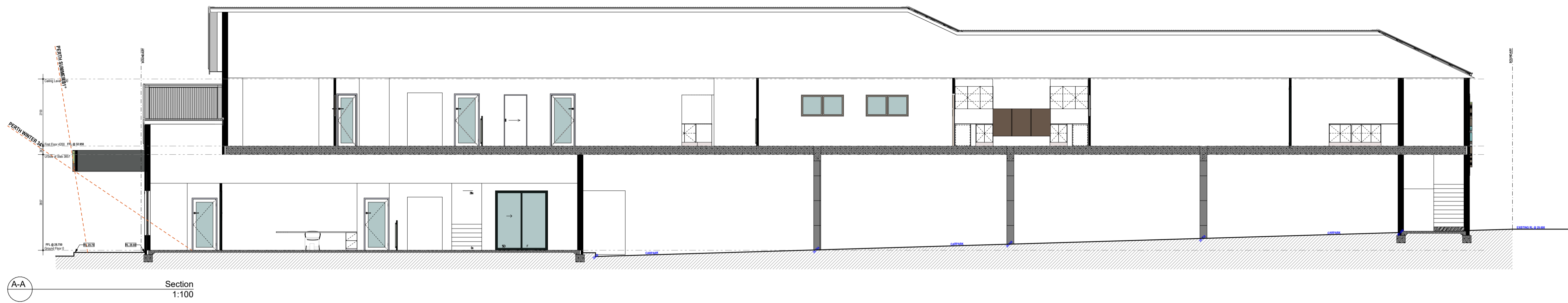
Scale:	Sheet Size:
1:100	A1
Project No:	Revision Number:
22082	7.00

Drawing No.:
12 of 14

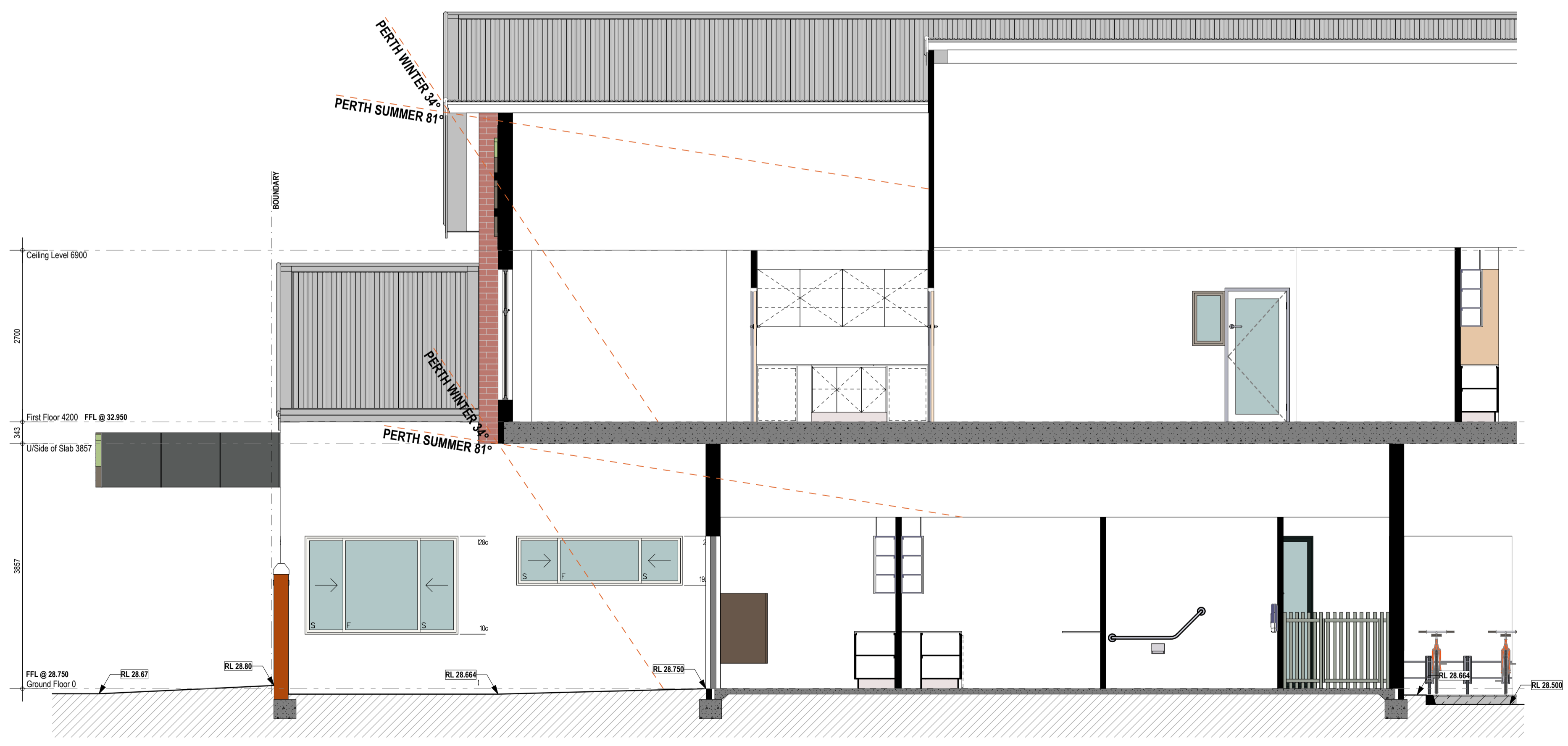


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A-A Section
1:100



B-B Section
1:50

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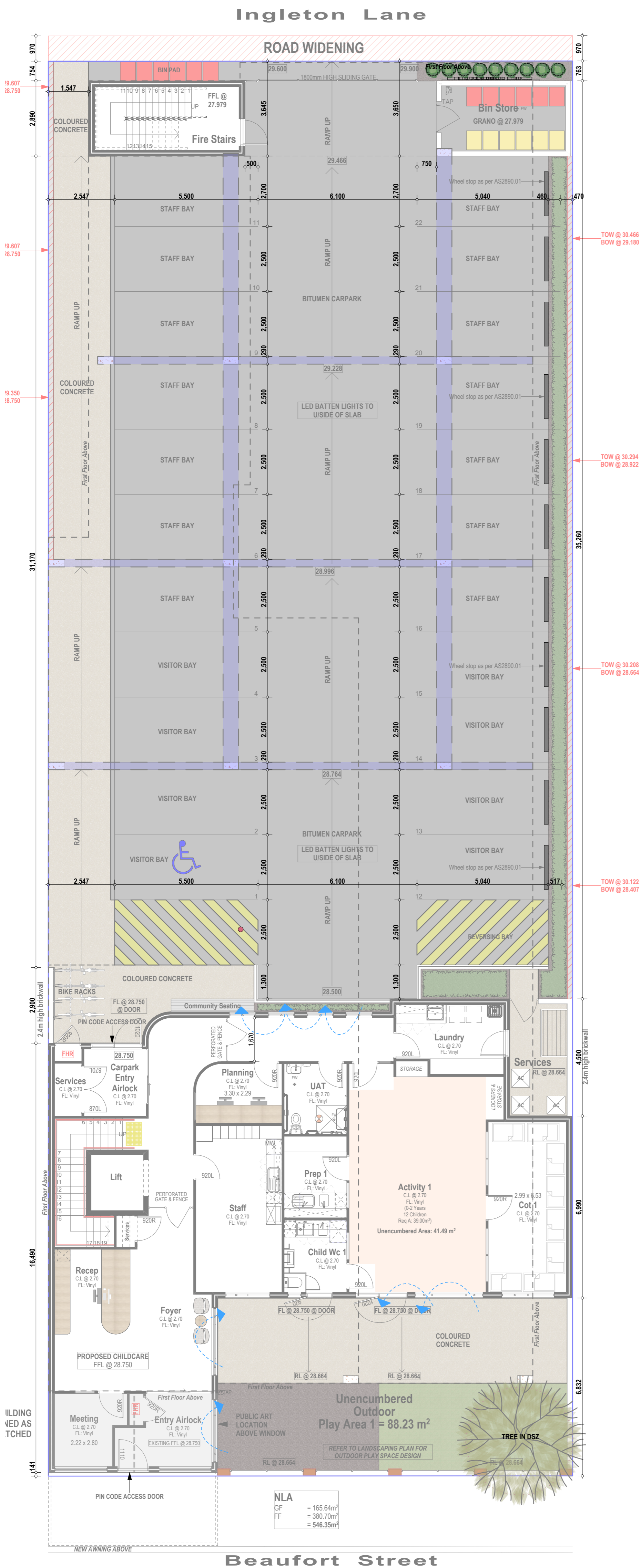
Project Name:
Childcare Centre
Project Address:
Lot 112 (#752) Beaufort St, Mt Lawley
Drawing Title:
Section & Solar Study

Scale:	Sheet Size:	A1
1:100, 1:50	Revision Number:	7.00
Project No:	22082	

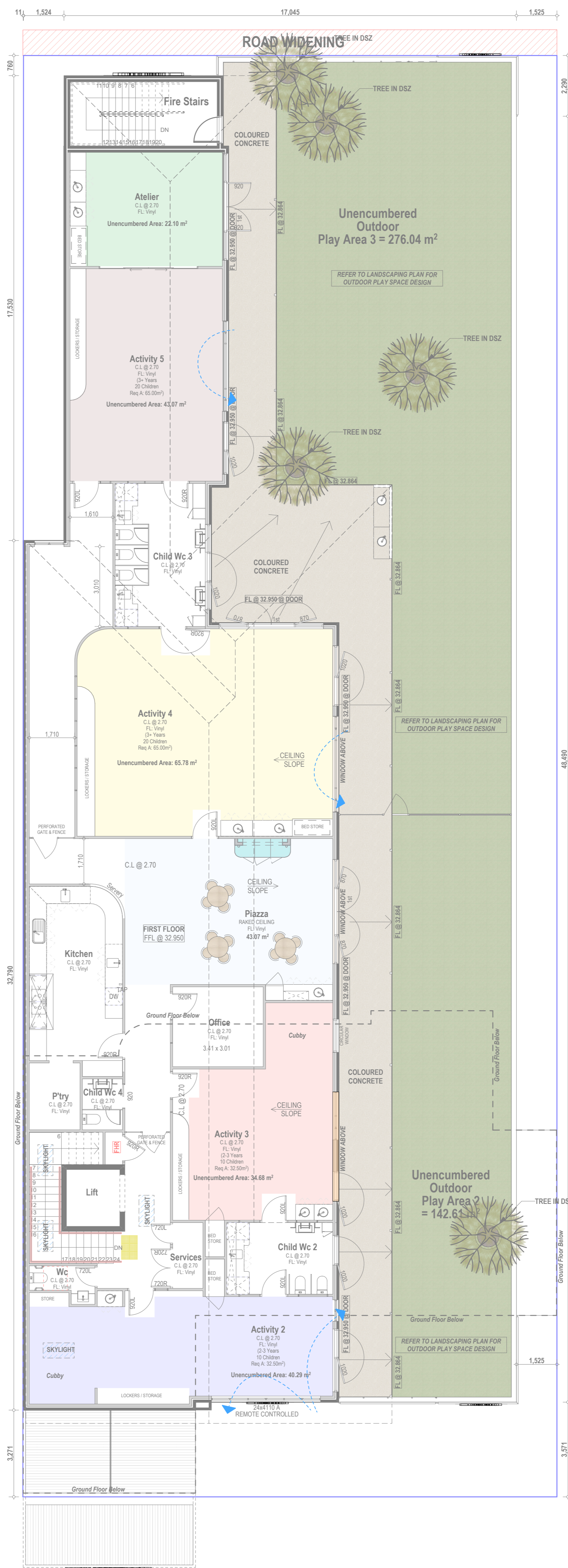
Drawing No.:
13 of 14

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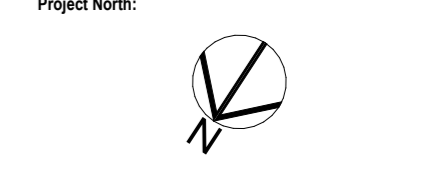


GF Breeze Path Plan
1:100



FF Breeze Path Plan
1:100

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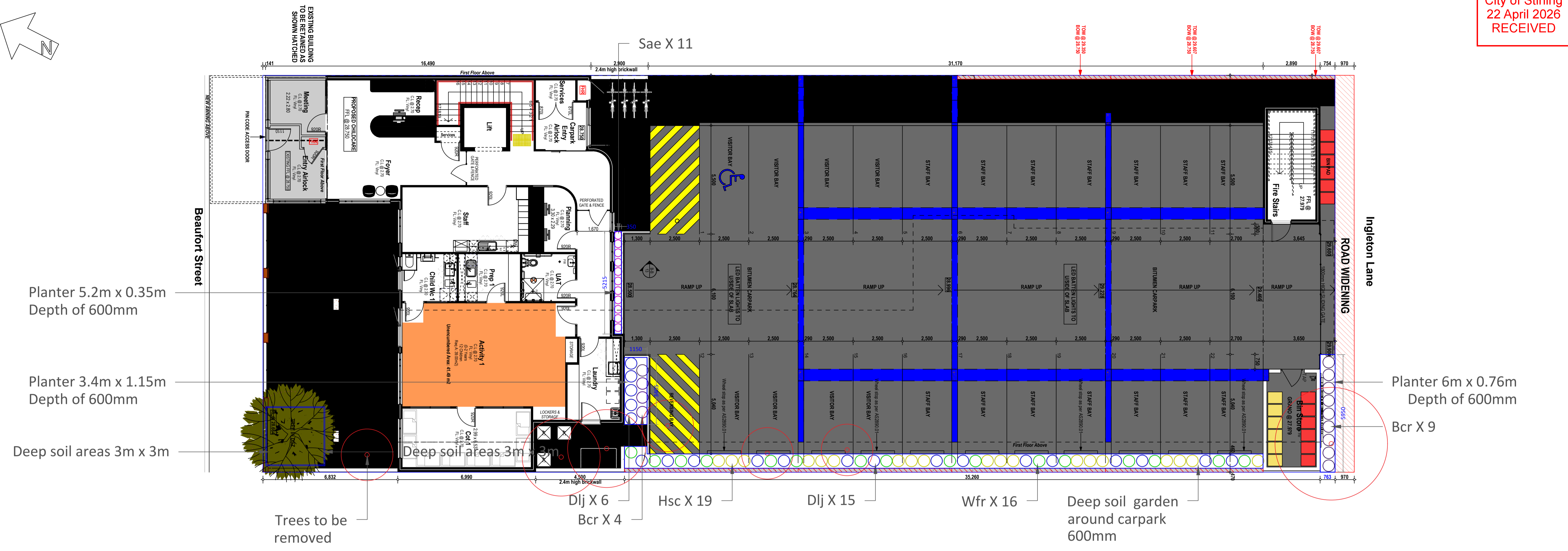
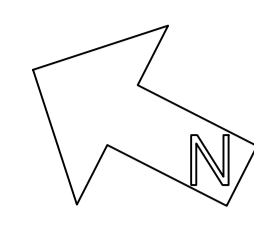
Project Address
Lot 112 (#752) Beaufort St, Mt Lawley

Drawing Title:
Breeze Path Plan

Scale: **1:100** Sheet Size: **A1**

Project No: **22082** Revision Number: **7.00**

Drawing No.:
14 of 14

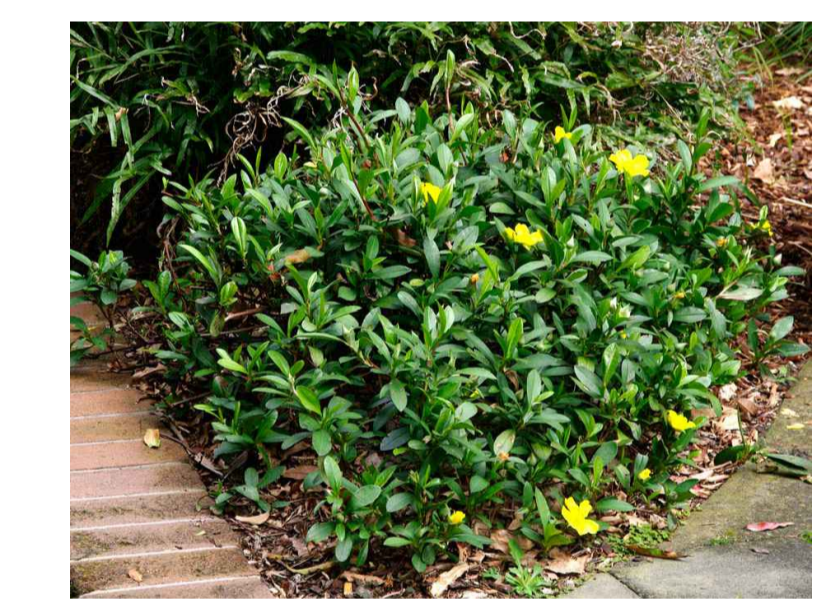


SHRUBS:

PLANT SCHEDULE					
	SPECIES	SPACINGS	SIZE	MATURE SIZE	QTS
SHRUBS					
Bcr	BORONIA CRENULATA	600 MM	140 MM	1 M	13
Hsc	HIBBERTIA SCANDENS	600 MM	140 MM	1 M	19
Dlj	DIANELLA 'LITTLE JESS'	600 MM	140 MM	0.4 M	21
Sae	SCAEVOLA AEMULA	400 MM	140 MM	0.5 M	11
Wfr	WESTRINGIA FRUTICOSA	600 MM	140 MM	1.5 M	16



DIANELLA 'LITTLE JESS'



HIBBERTIA SCANDENS



WESTRINGIA FRUTICOSA



BORONIA CRENULATA



SCAEVOLA AEMULA



P.O. Box 774
e-mail: admin@childscapes.com.au
web: childscapes.com.au

PROJECT 752 BEAUFORT ST
MOUNT LAWLEY
ground floor

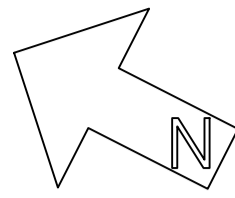
DRAWING PLANTING PLAN

DRAWING NO	SCALE	SHEET	REVISION
L-03	1:100	A1	3

ISSUE ISSUED FOR FEEDBACK

DRAWN	DATE
NK	16/04/26

REVISIONS			
No	DATE	DRAWN	DETAILS
0	10/11/25	NK	PLANTING- ISSUED FOR FEEDBACK
1	23/03/26	NK	PLANTING- ISSUED FOR FEEDBACK
2	15/04/26	NK	PLANTING- ISSUED FOR FEEDBACK
3	16/04/26	NK	PLANTING- ISSUED FOR FEEDBACK



PLANT SCHEDULE					
	SPECIES	SPACINGS	SIZE	MATURE SIZE	QTS
TREES					
Ope	ORNAMENTAL PEAR	AS SHOWN	90L	8 M	1
Mtr	MANDARIN TREE	AS SHOWN	90L	3-6 M	2
Ltr	LEMON TREE	AS SHOWN	90L	3-6 M	2

TREES:



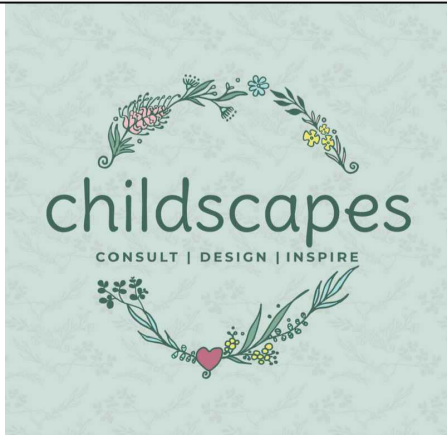
MANDARINE TREE



ORNAMENTAL PEAR



LEMON TREE



P.O. Box 774
e-mail: admin@childscapes.com.au
web: childscapes.com.au

PROJECT 752 BEAUFORT ST
MOUNT LAWLEY
first floor

DRAWING PLANTING PLAN

ISSUE ISSUED FOR FEEDBACK

REVISIONS			
No	DATE	DRAWN	DETAILS
0	15/04/26	NK	PLANTING- ISSUED FOR FEEDBACK
1	16/04/26	NK	PLANTING- ISSUED FOR FEEDBACK

CLIENT	DRAWING NO	SCALE	SHEET	REVISION	DRAWN	DATE
-	L-04	1:100	A1	1	NK	16/04/26



Proposed Childcare Centre at
752 Beaufort Street, Mt Lawley
Heritage Impact Statement

Prepared for

City of Stirling

By



HOCKING HERITAGE + ARCHITECTURE

October 2025

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Cover Illustration:

Figure 1 752 Beaufort Street, Mt Lawley
Hocking H+A, May 2025

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HHA Job No. 2025-21

Rev No	Author	Reviewer	Date
A	Renaë Canterbury		24 October 2025



1. Heritage Listings

752 Beaufort Street, Mt Lawley, is not on the City of Stirling heritage list or heritage survey. It does however fall within the Mt Lawley Heritage Protection Area (HPA), Mt Lawley East, which is an area of considerable significance. The site also falls within the Beaufort Street Local Development Plan Area, in particular the Second Avenue Mixed Use Precinct and the First Avenue Residential Precinct.

The works include the partial demolition of the existing building and the construction of a new childcare centre with carpark.

This heritage impact statement seeks to assess the proposed works in terms of impact on the heritage character of the area. The report has been prepared following the guidelines established by the Heritage Council in their publication *Heritage Impact Statement – A Guide*.

2. Site Analysis

2.1 Location

The property is located at 752 Beaufort Street, Mount Lawley. It can be found on Lot 112 on Plan Diagram 2473. The front of the property faces Beaufort Street with Ingleton Lane at the rear. The surrounding streetscape are generally single storey mixed use developments including retail and commercial to the east and residential properties on the north, south and west side.



Figure 2 Location Map
Courtesy Nearmaps, accessed 1 October 2025



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3. Subject Property

The following images show the current condition of the existing building at 752 Beaufort Street, Mt Lawley.



Figure 3 Northwest view of existing building



Figure 4 Southeast view of existing building



Figure 5 Western view of existing building



Figure 6 Main shopfront of existing building

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The following images demonstrate the streetscape character in the vicinity of the proposal. The subject site is located on the eastern side of Beaufort Street, near the Second Avenue intersection. To the north, the adjacent site contains a shop front that has previously been renovated, with apartments behind. To the South are further multi storey unit complexes.



Figure 7 760 Beaufort Street



Figure 8 Rear view of 760 Beaufort Street



Figure 9 Side view of 760 Beaufort Street



Figure 10 745 Beaufort Street



Figure 11 762 Beaufort Street



Figure 12 755 Beaufort Street

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Figure 13 63 Second Avenue



Figure 14 62 Second Avenue



Figure 15 771 Beaufort Street



Figure 16 776 Beaufort Street



Figure 17 771 & 777 Beaufort Street

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4. City of Stirling Planning Policies

The site falls within the City of Stirling's Heritage Protection Area (HPA), Mt Lawley East. This is considered to be an area of considerable significance.

The proposal has been assessed against the objectives of the Guidelines. These being:

1. Ensure the retention of buildings within the Heritage Protection Areas dating from the early 1900s to the 1950s where the architectural style of the building is generally intact;
2. Ensure that new buildings, alterations and additions to existing buildings, carports, garages, and front fences are in keeping with the heritage character of the area, respects the scale and proportions of surrounding buildings, and are designed to fit into the existing streetscape;
3. Maintain and improve existing street trees, grass verges and front gardens;
4. Retain mature trees wherever possible; and
5. Provide a framework for the assessment of development applications in line with the above points.

The site also falls within the Beaufort Street Local Development Plan and has been assessed against its contents.

5. Proposed Development

The works involve the partial demolition of the existing building on Lot 112 and the construction of a new childcare centre with carpark.



Figure 18 Proposed Beaufort Street elevation
Courtesy Germano Designs

Refer to the accompanying drawings prepared by Germano Designs for the full scope of works.



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6. Heritage Character of the Area

The following character statement is taken from the Beaufort St Development Plan:

The Second Avenue precinct, from Second Avenue to Third Avenue, which includes the recent development of the new supermarket and shops, together with existing shop buildings, have consolidated the Second Avenue Precinct into a strong local centre, the character of which is to be intensified, and the range of commercial activities expanded.

6.1 Beaufort Street, Mount Lawley Streetscape in the vicinity of Second Avenue

Beaufort Street Mount Lawley is a major arterial road heading northeast from the Perth CBD. For most of its length, it is a single-carriageway, two-way road with two lanes in each direction.

Beaufort Street was first established in the 1870s and provided access to the farming properties north of Perth. With the establishment of the suburb of Mount Lawley in the 1890s, generous residences were built on some of the larger lots at the southern end of Beaufort Street. However, Beaufort Street largely developed as a commercial strip catering for the growing population and was the location of a tram line from the city.

Most development in the area north of First Avenue occurred in the Inter War period and comprised single storey shops with residences to the rear. Generally, these shops were on the front property boundary and included awnings over the footpath. Shop premises typically clustered on the junctions of major roads such as Second Avenue and Beaufort Street. Sewerage Plans of the area dated 1915-1934 illustrate how the shops were small scale and regularly located on corner sites. Beaufort Street between Second Avenue and Third Avenue was the location of most of the commercial premises in this area.

The premises on the diagonally opposite corners of Second Avenue and Beaufort Street are isolated examples of the original construction. The Second Avenue Deli and adjacent shop (HNs 745 and 751); and The Rock Inn (HN762) show some evidence of commercial construction from this period.



Figure 19 Intersection of Beaufort Street and Second Avenue looking south



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The majority of the built form in the study area demonstrate a diverse range of styles constructed throughout the second half of the 20th century and some more recent examples from the 21st century which have referenced the Inter War styles. Most prominently the Second Avenue Plaza shopping complex with its prominent curved façade addressing the northeast corner of Beaufort Street. The three storey mixed use premises on the southwest corner of Beaufort Street and Second Avenue used the bricks from the original shop premises and maintained the scale of the original commercial premises.

A feature of this area of Beaufort Street are blocks of brick flats from the mid-20th century. Astora (HN746), Myola (HN744) and Vasto Gardens (738-742) demonstrate styles of flat development typical of the 1940s to the 1970s. Although they have a similarity of scale and location on their lots, these three blocks of flats have no uniformity of style or detail. The two-storey brick residential development directly opposite this group on the northern side of Beaufort Street is typical of unit development of the late 20th century which mimics Federation detailing. The two storey brick premises at 771 Beaufort Street now used for commercial purposes were flats typical of the post war period.

The built form north of Second Avenue has greater evidence of development from the Inter War period. Good Sammys (HN768) has a parapet demonstrating influences from the Inter War Stripped Classical style although the shop front has been significantly altered by later additions. Masonic Charity Outlet (HN772) is a largely intact shopfront, and the adjoining former residential premises set back from the street demonstrates Federation style and detail.

The commercial premises at 769 Beaufort Street although demonstrating details from the Federation period was constructed in the late 1950s and is therefore a reproduction of an earlier style.

Overall, the Beaufort Street streetscape between First Avenue and Third Avenue demonstrates a range of architectural building styles, materials and details. There is no cohesive built form as the remnants of the original development of the area are now largely separated by later developments of varying forms and materials.

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7. Assessment of Impact

The proposed development has been assessed against the statement of significance for the Mount Lawley (East) Heritage Protection Area.

Statement of significance	Impact
Mount Lawley (East) is a good example in Perth of a substantially intact residential area characterised by large and medium sized homes from the early twentieth century and inter-war period, many of high architectural quality, in established landscaped gardens.	The proposed works have no impact to the residential area of Mount Lawley (East).
Typical example of rectangular grid road and subdivision pattern;	No impact, the proposal is not seeking any changes to the subdivision pattern.
Good examples of housing from the early 1900s and inter-war period including some very good examples of Federation Bungalow and Californian Bungalow style houses;	The retention of the front section of the original shop is positive. The removal of the rear section will have some adverse impact.
Street design including the general street layout including grass verges and street trees;	No impact.
Garden layout, design and quality.	Positive impact. Garden spaces have been incorporated in the new development

The proposed development has been further assessed against the objectives of the City's heritage policy, as well as the Beaufort Street Local Development Plan.

Demolition

The proposal seeks approval to demolish the rear portion of the existing shop, retaining the front 3m. Documentary evidence shows that the existing building was constructed c1920 and has undergone numerous modifications over the years. The current shop front is not original, and the interior retains very little of the original layout, if any.

The original plans for the site, included below, show a central recessed entrance.

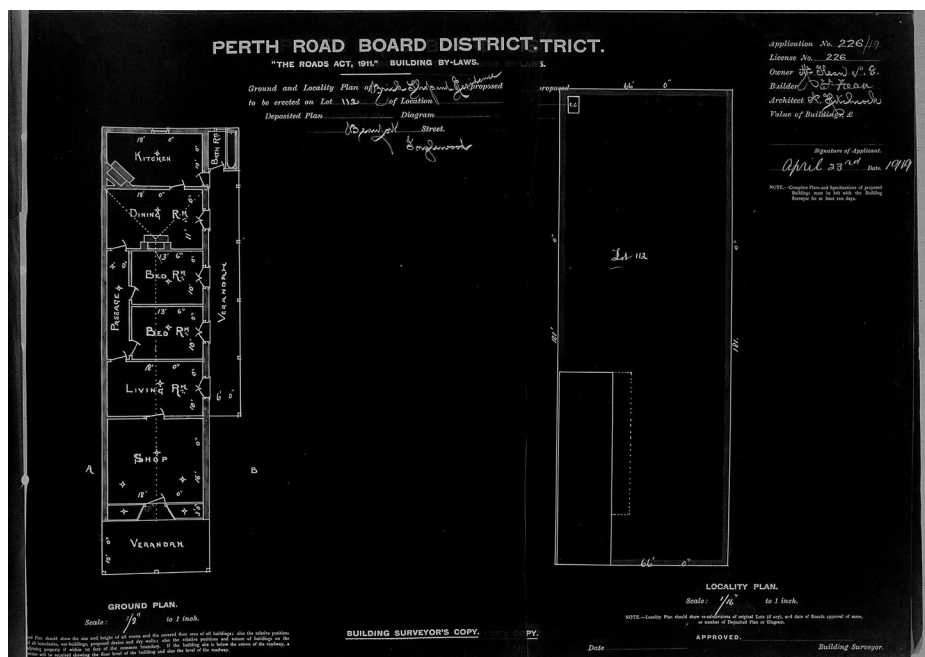


Figure 20 Proposed plans from 1919 for 752 Beaufort Street
Courtesy SLWA

The following images are courtesy of Google Streetview and show various modifications to the shop front since 2007.

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Figure 21 November 2007



Figure 22 March 2014



Figure 23 February 2015

While the shopfront and interior of the original shop have little authenticity, the original footprint of the building generally remains intact.

The retention of only the façade of the existing shopfront will have an adverse impact, as the original form and scale will be lost. The removal of the rear section does however allow for the reactivation of the site and the conservation works to the heritage façade.

The first 6.8m of the ground floor of the new development is aligned with the existing southern wall, meaning that the front of the development reflects the original form. Where the first floor aligns, the original walls in these locations could not be retained for structural reasons. The interpretation of the original shop exterior walls for the first 6.8m is seen as a positive compromise to the partial demolition.

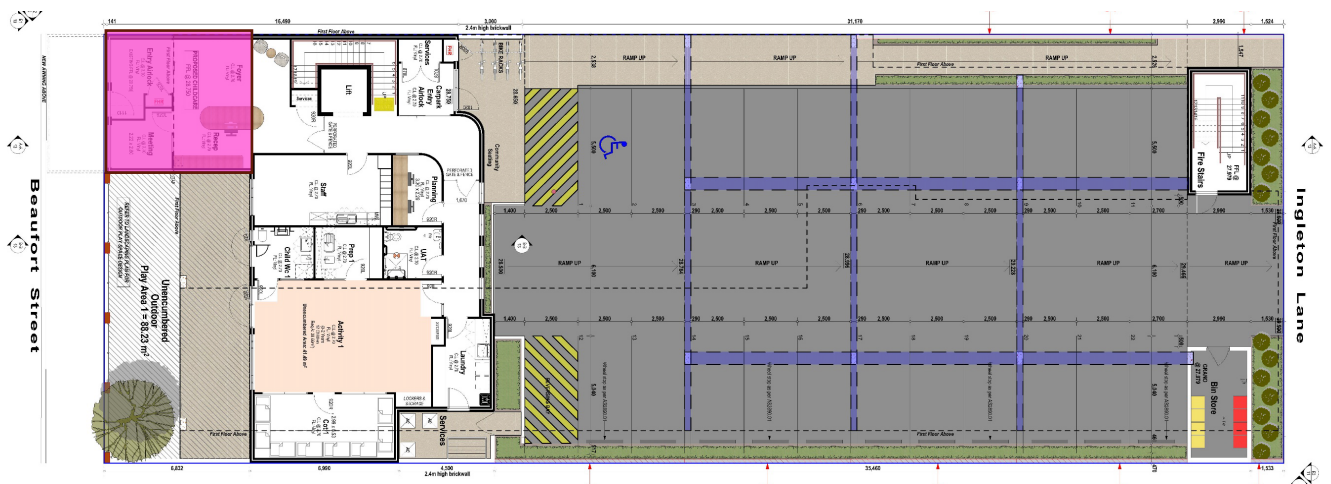


Figure 24 Interpretation of original shop footprint shown in pink.
Courtesy Germano Designs

The proposed demolition of the rear section of the shop has previously been discussed with the City of Stirling and their in-principal support for retention of only the front 3m has been provided.

Change of Use

The proposed change of use from a shop to a childcare centre will have minimal impact to the heritage significance of the place and streetscape as it retains a commercial use. The change of use will however reactivate the existing building which has been vacant for some time and is seen as a positive outcome.

New buildings and additions

Under the Development Plan,

All redevelopment of existing pre 1960's buildings shall be in keeping with its original pre-1960's architectural style.

The existing shop does not fit in one of the four identified styles by the Development Plan or Guidelines. The building is Inter-War and given its aesthetic could be considered to be Inter-War Stripped Classical. These buildings embrace simplicity. The style demonstrates a basic classical composition while eliminating traditional decorative detailing. Buildings are usually symmetrical and well-proportioned like other classical styles. The proposed addition reflects this philosophy in a modern way and is simplistic in appearance.



The scale of the first-floor addition reflects the adjacent streetscape. The ridge height is reflective of the adjacent properties, as demonstrated in the elevation below.

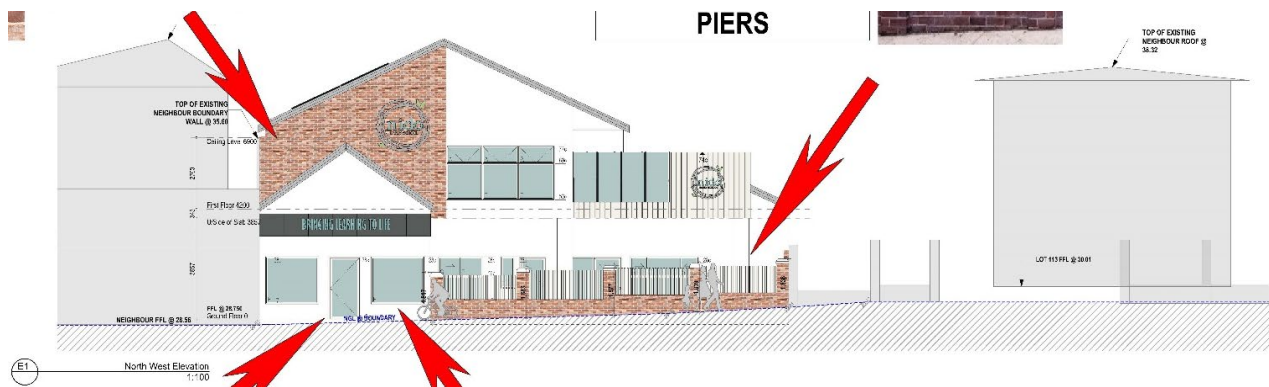


Figure 25 Beaufort Street elevation showing adjacent properties.
Courtesy Germano Designs

The first-floor addition is set back to ensure the original shopfront is clearly identifiable.



Figure 26 Street view showing first floor setback and alignment of ground floor
Courtesy Germano Designs

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Shopfront

Under the Beaufort Street Local Development Plan;

Refurbished shop fronts are to be designed to ensure that the original shop fronts are re-instated with original materials and details;

To ensure new shop fronts are designed to match one of the frontage styles and use traditional materials;

All new and restored shop fronts shall be constructed of traditional materials, such as timber window frames and pressed tin ceilings.

There is no documentary evidence to show that the proposed shop front, as drawn in the 1919 plans was constructed. The proposed shopfront has a central door flanked by large windows. The shopfront is symmetrical, reflecting the original building plans. The window sills and head heights are reflective of traditional shop fronts. The proposed use of wide profile aluminium framed windows and door ensures the shopfront is readily identifiable as new. Given the lack of historical evidence as to what was actually constructed and the numerous changes over the years, this is considered acceptable.

The existing render to the shop front is retained, as there is insufficient documentary evidence as to the original materials and details. This is considered an acceptable heritage outcome and painted render is reflective of a traditional material palette.

The new awning replaces the existing awning, to the same geometry. The awning seeks to utilise sheet cladding with a vertical joint. The use of a modern material for the awning will have a minor adverse impact on the heritage streetscape, however given the lack of cohesion in this section of Beaufort St, the impact is considered acceptable.

Roof

The main roof pitch is 25 degrees, which is in keeping with traditional roof pitches. The roof of the shop is maintained for the first 3m.

Material Palette

The proposal uses a combination of recycled bricks, painted render and a zincalume roof, which is reflective of a traditional material palette. Vertical seamed cladding is proposed for the walls of the first-floor outdoor play space. While not a traditional material, its use does not detract from the wider streetscape.

While the window and door frames do not use timber, a wider profile is proposed to better reflect the geometry of the traditional timber framing. The use of aluminium for maintenance reasons, is considered to have a minimal adverse impact, and is reduced through the use of the wider profile. This aligns with the Guidelines and is considered acceptable.

The proposed materials are reflective of the material palette in the wider streetscape.

Fence

The front fence has been designed to reflect the scale and rhythm of traditional fencing. The face brickwork is reflective of the existing heritage material palette and the white fence pickets are in keeping with traditional colours.

Landscaping

Additional landscaping is proposed for the carpark and outdoor play areas.

Trees

A new tree is proposed in the front setback.



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8. Conclusion

The proposed childcare centre at 752 Beaufort Street, Mt Lawley, utilises an existing empty shop. The proposal has been designed to sit comfortably within the streetscape.

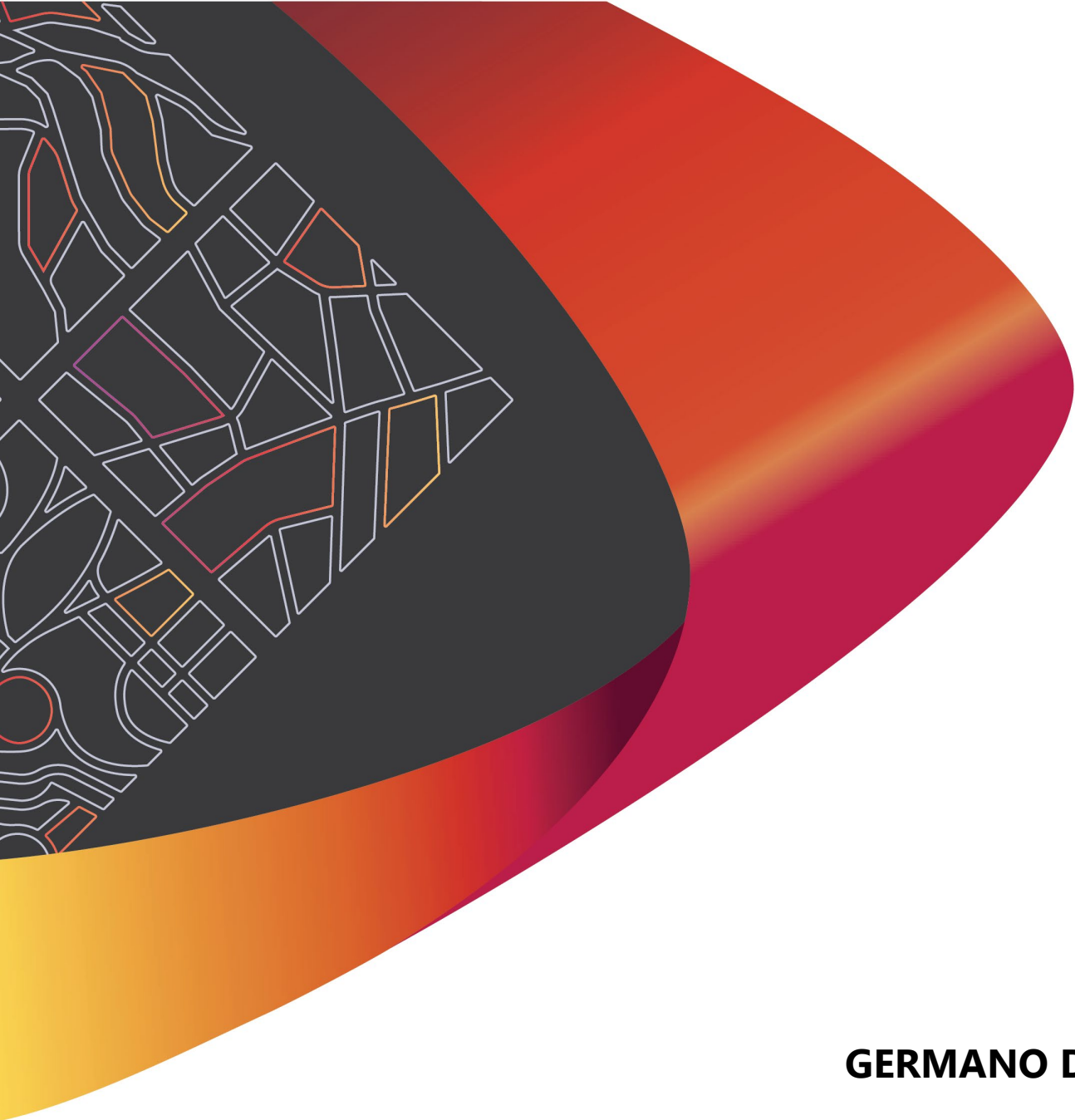
Overall, the Beaufort Street streetscape between First Avenue and Third Avenue, where the subject site falls, demonstrates a range of architectural building styles, materials and details. There is no cohesive built form as the remnants of the original development of the area are now largely separated by later developments of varying forms and materials.

The proposal does not adversely impact any of the key values and characteristics of the wider heritage area, the adjacent properties or the streetscape.

The proposed childcare centre aligns with the key objectives of the City of Stirling's Character Retention Guidelines Mt Lawley and Menora, as well as the Beaufort Street Development Plan.

With consideration of this, the proposed works are recommended for approval.

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GERMANO DESIGNS

LOT 112 (#752) BEAUFORT STREET, MOUNT LAWLEY
TRANSPORT IMPACT STATEMENT

Job No: P004693

Rev A

24 February 2026



Premise

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1. EXECUTIVE SUMMARY

Site Context

- > The project location is Lot 112 (#752) Beaufort Street, Mount Lawley within the City of Stirling.
- > The site has frontage to Beaufort Street to the northeast and Ingleton Lane to the southwest. It falls within the area of the Beaufort Street Local Development Plan and Character Retention Guidelines Mount Lawley and Menora.
- > At present the site features an Indian restaurant that is to be demolished and replaced with a childcare centre. The childcare centre will cater for up to 72 children and will have a maximum of 13 staff at any given time.

Technical Findings

- > The proposed development will generate up to 214 vehicular trips per day, including 58 vehicular trips per hour in AM and 58 vehicular trips per hour in PM peak.
- > According to the WAPC Guidelines, this is a moderate impact on the surrounding road network.
- > No service vehicle access or loading facilities are proposed along the Beaufort Street frontage. Service vehicles will not enter the subject lot, instead, they will access Ingleton Lane and undertake waste collection from the laneway adjacent to the site. As noted in Section 6.1.5 of the LDP, the laneway will be widened to 6.0 metres, which will facilitate forward entry and exit manoeuvres for service and commercial vehicles.

Relationship with Policies

- > The plans show 22 on-site car parking bays, while the statutory requirements vary under different documentation. Premise completed operational assessment of the proposed parking provision and believes that the practical parking demand for the proposed childcare centre is satisfied with the provision. This is elaborated in detail in **Section 4.2** of this report.
- > The proposed development requires one (1) ACROD parking bay in accordance with the Building Code of Australia. Plans show one (1) ACROD parking bay in line with the requirements.
- > The proposal includes the provision of 4 dedicated bicycle parking bays for use by staff or visitors, which is a surplus to City of Stirling's LPP 6.7 – Parking and Access requirements for 2 bicycle bays. This provision is considered more than sufficient to cater to any potential demand.

Conclusion

- > This Transport Impact Statement concludes that the traffic and parking generated by the proposed childcare centre at Lot 112 (#752) Beaufort Street, Mount Lawley, can be accommodated by the existing and planned road network without any adverse impacts on safety or efficiency.
- > The proposed parking supply and access arrangements are compliant with all relevant statutory requirements. Therefore, it is concluded that the development is supportable on traffic and transport planning grounds.

2. INTRODUCTION

2.1 Background

Premise Australia Pty Ltd (**Premise**) has been engaged by Germano Designs to prepare a Transport Impact Statement (TIS) for the proposed development at Lot 112 (#752) Beaufort Street, Mount Lawley within the City of Stirling.

The proposed development will comprise a childcare centre accommodating up to 72 children and 13 staff members.

2.2 Scope and Study Area

This report outlines the traffic impact statement for the proposed childcare centre.

The purpose of this assessment is to evaluate the suitability of the site for the intended land use from a traffic impact perspective, taking into account local transport networks, safety concerns, and relevant regulatory requirements.

The scope of work for the Traffic Impact Statement is as follows:

- > Collate all existing traffic data for relevant traffic networks in the vicinity of the subject site.
- > Undertake a detailed review of crash data between in the last five (5) year reporting period and provide commentary on the road safety aspects of the data and potential reasons for the number and type of incidents.
- > Provide an assessment of the likely additional traffic impact of the proposed development.
- > Review all existing public transport routes, pedestrian and cyclist infrastructure, and show graphical images overlaid on aerial imagery within 400-metre radius of the subject site.
- > Calculate trip generation for AM / PM peak and daily traffic based on the proposed yield and land use.
- > Provide a report according to the set-out requirements as nominated in the WAPC Transport Impact Assessment Guidelines: Individual Developments
- > Provide further analysis of any site-specific issues that may be encountered during the assessment.

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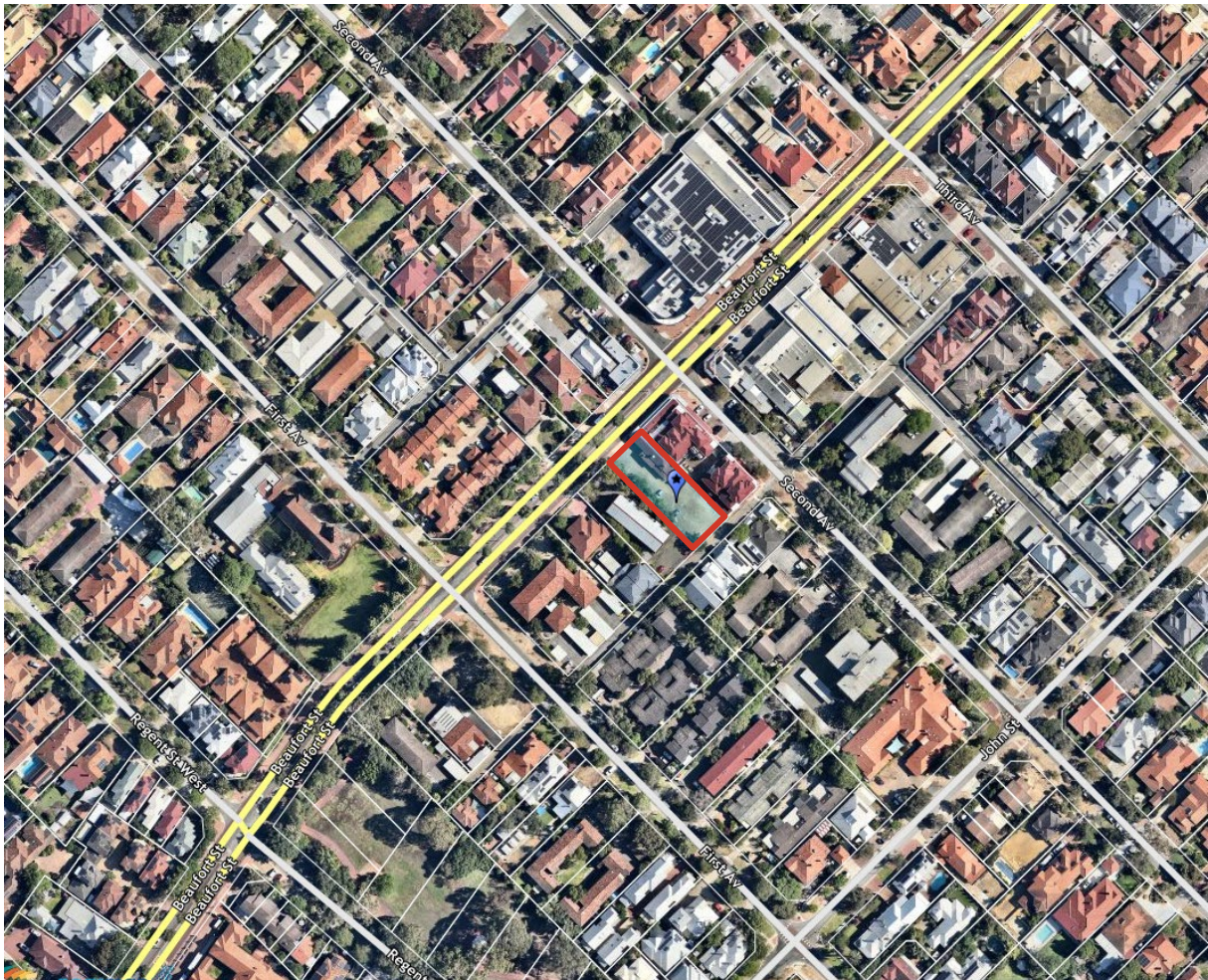
3. EXISTING CONDITIONS

3.1 Site Location and Description

The subject site comprises Lot 112 (#752) Beaufort Street, Mount Lawley, located within the suburb of Mount Lawley in the City of Stirling. The site has frontage to Beaufort Street to the northwest and Ingleton Lane to the southeast.

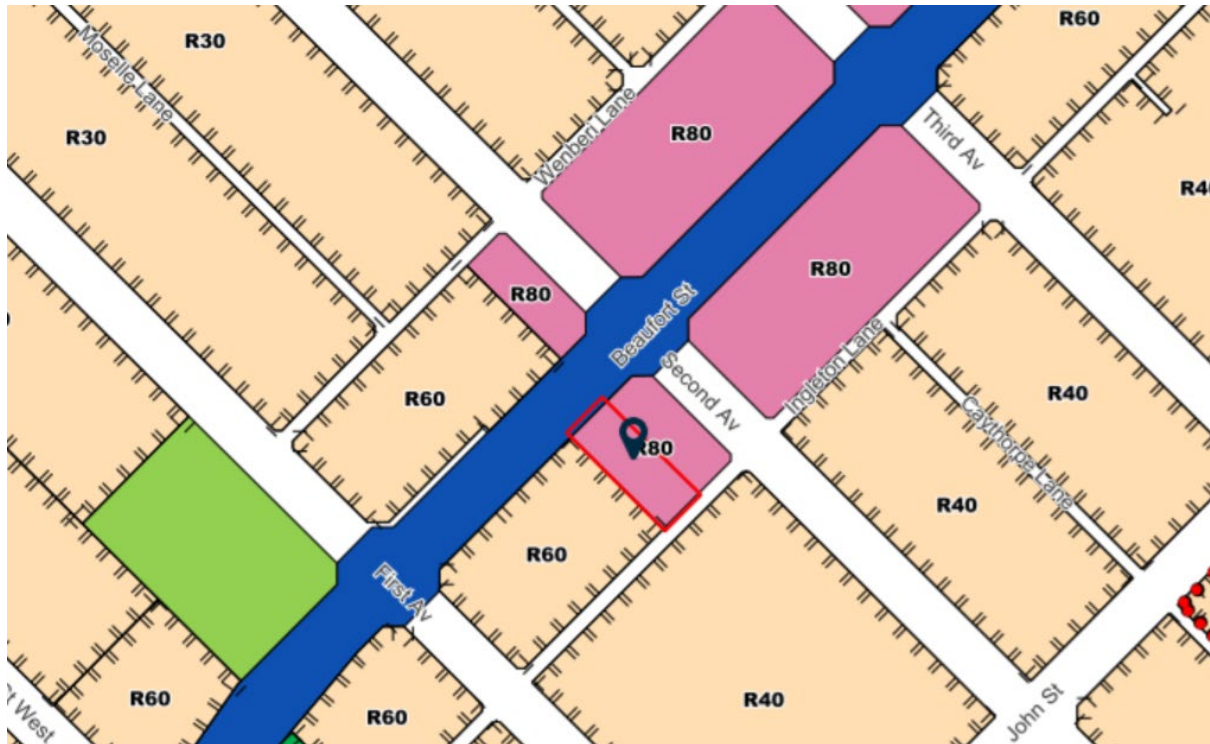
The site is currently features hospitality premises (a restaurant), which are to be demolished to facilitate the proposed development.

Figure 1 - Site Location



Under the City of Stirling’s Local Planning Scheme No. 3 (LPS3), the site is zoned as ‘Mixed Use’ and falls within the area of the Beaufort Street Local Development Plan and Character Retention Guidelines Mount Lawley and Menora. The surrounding area is characterised by residential and other mixed-use developments.

Figure 2 - Site Zoning



3.2 Existing Road Conditions

Table 1 – Road Classification and Description

Road Name	Beaufort Street
Number of Lanes	two way, one lane per direction, divided
Road Reservation Width	approximately 20m
Road Pavement Width	approximately 12m
Classification	Distributor A
Speed Limit	60kph
Bus Route	NO
If YES Nominate Routes	67, 68, 950, 950X
On-street parking	YES

Road Name	First Avenue
Number of Lanes	two way, one lane (no linemarking), undivided
Road Reservation Width	approximately 20m
Road Pavement Width	approximately 6m
Classification	Access Road
Speed Limit	40kph
Bus Route	NO
On-street parking	NO

Road Name	Second Avenue
Number of Lanes	two way, one lane (no linemarking), undivided
Road Reservation Width	approximately 20m
Road Pavement Width	approximately 6.7m
Classification	Access Road
Speed Limit	40kph
Bus Route	NO
On-street parking	YES

Road Name	Inlgeton Lane
Number of Lanes	two way, one lane (no linemarking), undivided
Road Reservation Width	approximately 5.4m
Road Pavement Width	approximately 5.4m
Classification	Access Road
Speed Limit	40kph
Bus Route	NO
On-street parking	NO

3.3 Review of the Available Planning Information

3.3.1 CITY OF STIRLING DRAFT LOCAL PLANNING SCHEME NO 4

The City of Stirling is currently finalizing Draft Local Planning Scheme No. 4 (LPS 4). A review of the draft scheme indicates that City of Stirling maintains that the development site as 'Mixed-use' and is currently progressing a Local Planning Policy (LPP) specific to the Beaufort Street corridor to replace the current Local Development Plan.

3.3.2 BEAUFORT STREET LOCAL DEVELOPMENT PLAN

The subject site falls within the boundary of the Beaufort Street Local Development Plan, which operates in conjunction with the City of Stirling Local Planning Scheme No. 3. The LDP establishes specific development standards aimed at revitalizing the corridor into a pedestrian-oriented urban environment. It is noted that this LDP will be replaced by the LPP released in conjunction with LPS4.



Figure 3 - Beaufort Street LDP Area



3.3.3 HERITAGE PROTECTION AREA SPECIAL CONTROL AREA

The subject site is located within the Mount Lawley and Menora Heritage Protection Area.

Figure 4 - Heritage Protection Special Control Area

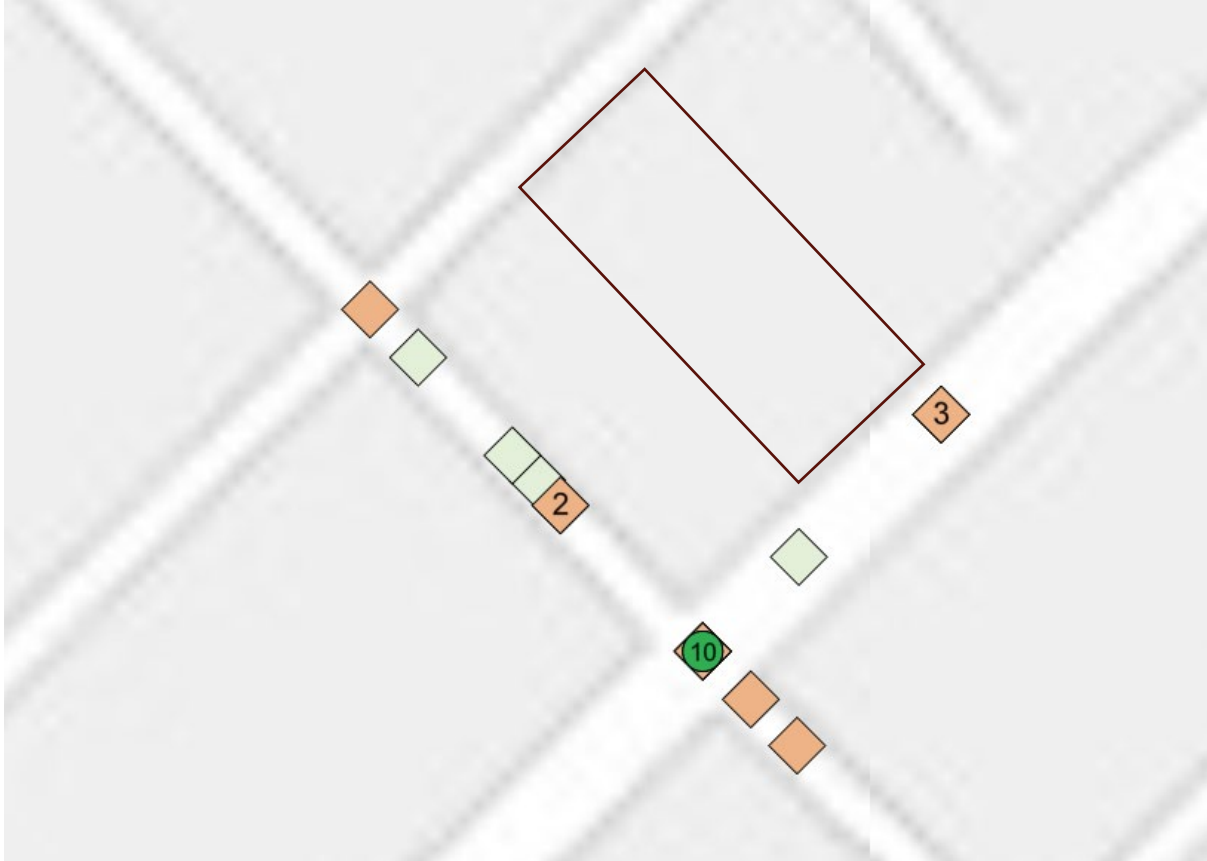


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3.4 Traffic Safety

A review of the MRWA database for all crashes along in the vicinity of the site has been carried out. The crash database currently provides the location and severity of all crashes for the five-year period from 2020 to 2024.

Figure 5 - Crash records - 2020-2024 in vicinity of the subject site



As anticipated, majority of the crashes occurred at Beaufort Street and Second Avenue intersection (total of 10 crashes with 3 crashes of medical severity). This cluster of crashes has high percentage of crashes relating to right turning movements (70% crashes), which is not unusual for a four-way sign-controlled intersection.

The analysis of the crash data shows that the intersection has 68.5 all crashes per 100 MKTV, which is slightly above the metropolitan intersection average (61.83). As the intersection had no KSI crashes recorded in the reporting period, the average is well below metropolitan intersection average (2.63).

Crashes along Second Avenue are predominantly related to parking manoeuvres. The only PDO crash that occurred at the intersection of Second Avenue and Wenberi Lane (in 2020) involved a vehicle reversing in the traffic.

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Table 2 - Crash Data (extract)

Location	Year	Severity	EVENT_NATURE
Second Av and Wenberi Lane	2020	PDO Major	Reversing (RUM 47)
Second Ave (SLK 0.87)	2020	PDO Minor	Leaving Driveway (RUM 45)
Second Ave (SLK 0.85)	2020	PDO Minor	Parking (RUM 44)
Second Ave (SLK 0.84)	2023	PDO Minor	Parking (RUM 44)
Second Ave (SLK 0.84)	2021	PDO Minor	Parking (RUM 42)
Second Ave (SLK 0.84)	2024	PDO Major	Parking (RUM 42)

The recorded crashes are limited in number and are generally distributed along Beaufort Street and nearby intersections, with event types including right-angle and rear-end collisions. The severity levels recorded (Medical and PDO Major) are consistent with typical urban arterial road conditions and do not suggest an abnormal safety concern specific to the site frontage.

Overall, the available crash data does not identify any existing road safety deficiencies associated with the subject site, nor does it indicate that the proposed development would exacerbate local crash risk, subject to compliance with standard access and traffic management requirements.

3.5 Existing Traffic Flow

The following table provides an overview of traffic count data for roads in the vicinity of the subject site. Refer to Appendix B for graphical representation of this data.

Table 3 – Traffic counts data

Road Name	Location of Traffic Count	Vehicles Per Day (VPD)	Vehicles per Peak Hour (VPH) Peak Time - Peak VPH		Heavy Vehicle %	Date
			AM	PM		
Beaufort Street	South of Grand Promenade (SLK 0.20)	24,763	07:45 – 1,817	16:30 – 2,048	6.0	2021 /22
	North of Regent Street (SLK 0.69)	18,303	07:15 – 1,211	16:45 – 1,306	4.0	2021 /22
	North of Lawley Crescent (SLK 0.55)	18,119	07:15 – 1,163	16:45 – 1,294	3.2	2021 /22
	North of Chelmsford Road (SLK 1.53)	19,921	07:45 – 1,490	16:45 – 1,357	7.4	2021 /22
Central Avenue	East of Alexander Drive (SLK 2.50)	16,624	07:45 – 1,486	14:45 – 1,443	4.1	2023 /24

**Note – These traffic counts were collected from the Main Roads Western Australia Traffic Map.*



3.6 Public Transport

The site is serviced by bus routes operating along Beaufort Street. The closest bus stop is named 'Beaufort St After Second Av' and is located approximately 100m from the site. The bus stop is within a walkable catchment (typically a 5-minute walk or 400m). Mount Lawley is the nearest train station to the site. However, the train station is located outside the typical acceptable walking catchment (10 min walk)

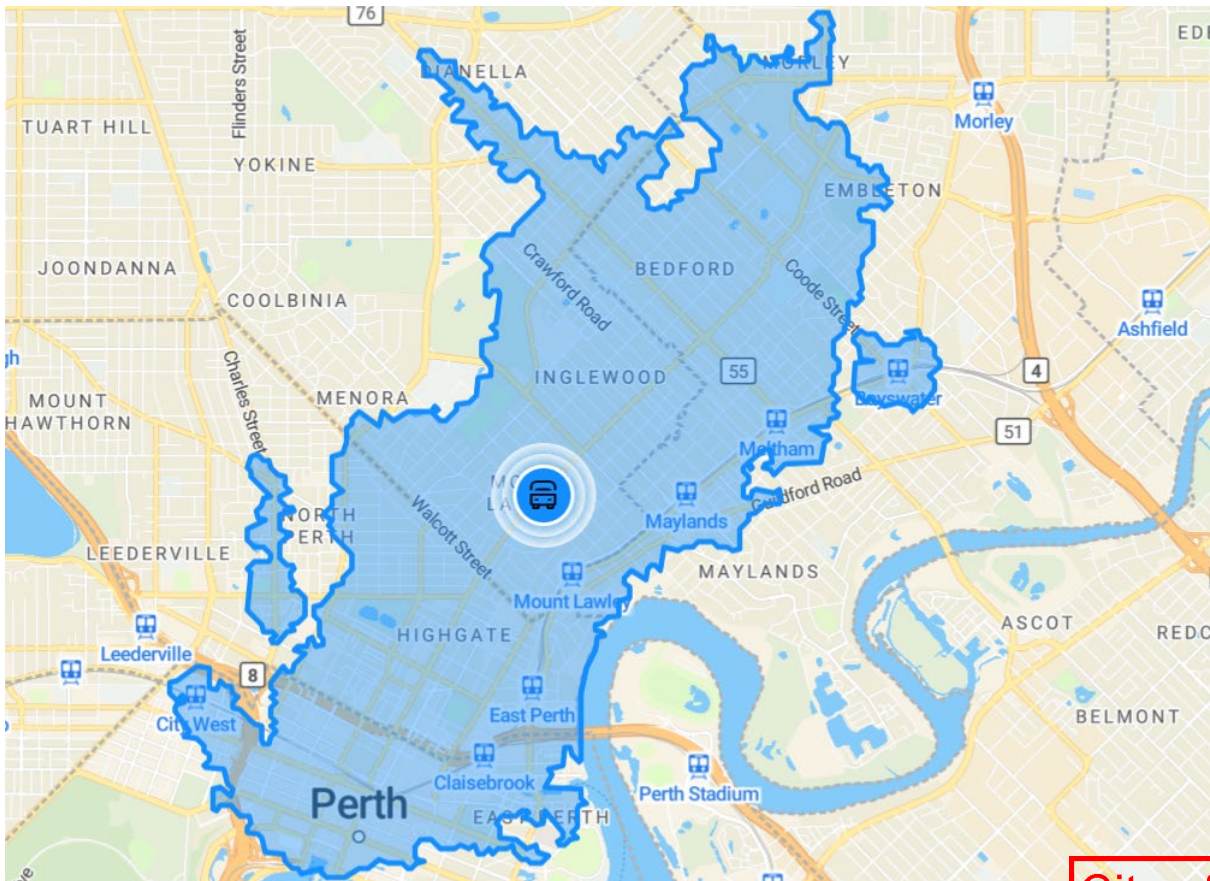
Table 4 - Bus routes and frequencies

Bus / Rail Route	Description	Peak Frequency	Off-Peak Frequency
67 and 68	Perth Busport – Mirrabooka Bus Station	Every 17 minutes	Every 60 minutes on weekends
950 and 950X	Morley Station – QEII Medical Centre	Every 6 minutes	Every 30 minutes

What is the Walk Score Rating?
 58 Many nearby public transportation options

Southbound buses from the site provide a direct and rapid link to the Perth CBD, terminating at or stopping adjacent to the Perth Busport. Given the site's proximity to public transport infrastructure, the development is well-positioned to achieve a higher non-car mode share than typical suburban childcare centres.

Figure 6 - 30min public transport catchment (app.traveltime.com)



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3.7 Pedestrian and Cyclist Infrastructure

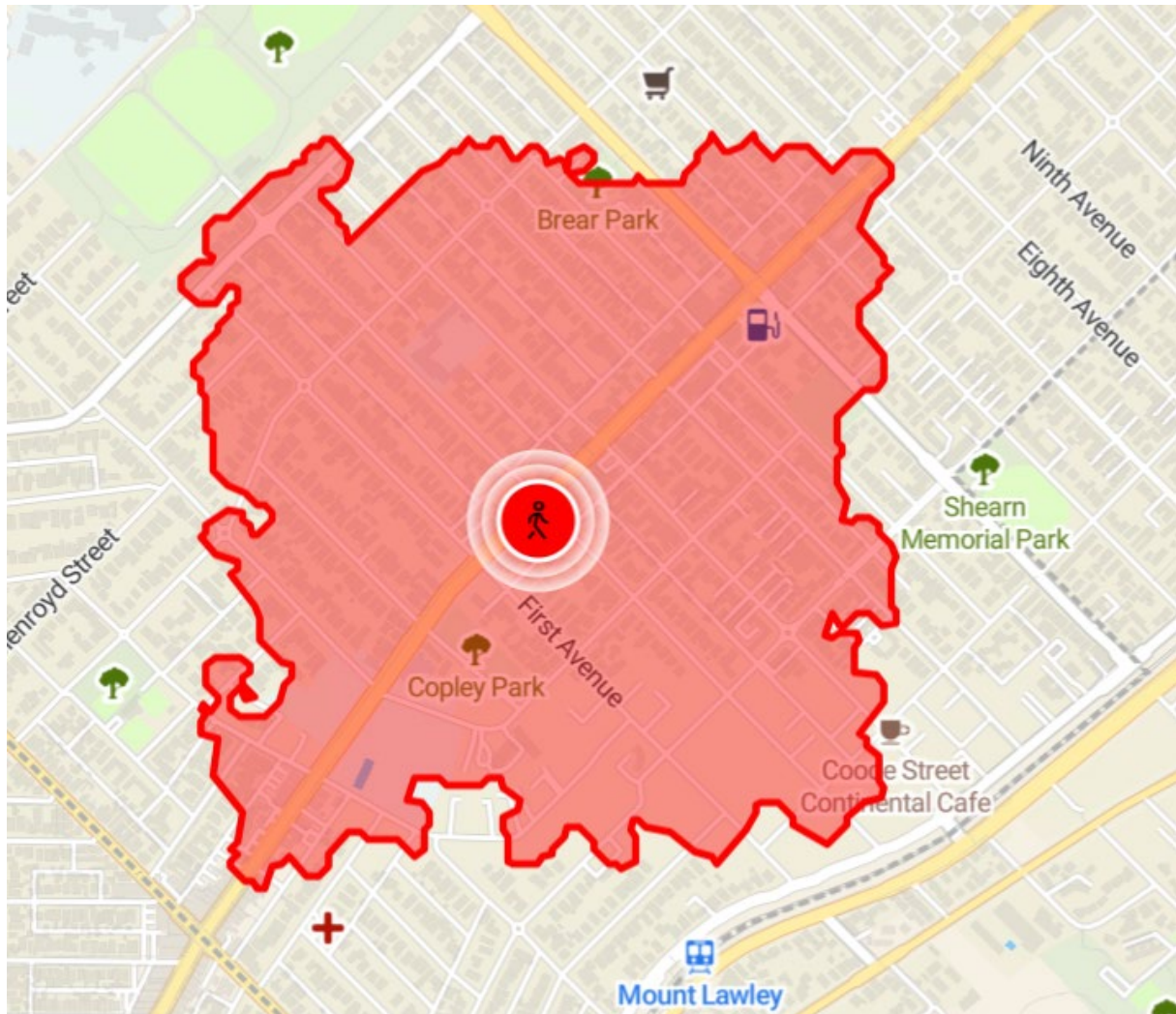
Premise have done a desktop review of the pedestrian and shared paths surrounding the subject lot. Refer to Appendix B for graphical representation of the below table.

Table 5 - PBN routes in the vicinity of the subject site

Classification	Road Name
<i>"Perth Bicycle Network (PBN) – Continuous Signed Routes"</i>	Fourth Avenue, Clifton Crest
What is the Walk Score Rating?	
81	Very Walkable. Most errands can be accomplished on foot.

While there is no dedicated cycling infrastructure directly fronting the site, there are significant PBN networks within vicinity of the site. The networks around the site connect to the Perth CBD and the broader transport network. It is expected that the proximity to the PBN will encourage a degree of active transport usage.

Figure 7 - 10min walking catchment (app.traveltime.com)



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Figure 8 - 15min cycling catchment (app.traveltime.com)



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4. PROPOSED DEVELOPMENT

4.1 Overview of Proposed Development

The development proposal includes a childcare centre that is proposed to accommodate a total of 72 children and is expected to employ 13 staff.

Table 6 - Proposed land uses and yields

Proposed Land Use	Yield
Childcare Centre	72 children
Staff	13 employees

4.2 Vehicular Parking

The City of Stirling’s Beaufort Street LDP Section 6.1.1 Car Parking Standard states that the car parking requirements for developments are to be in line with the extracted rates from the table below.

Table 7 - Car parking provision rates

Guideline document	Car parking requirement
City of Stirling’s Beaufort Street LDP Section 6.1.1 Car Parking Standard	Non-residential land use: 3.5 per 100m ² Net lettable area

Table 8 – Car parking requirement calculations

Land Use / Type	Yield	Required Parking
Children in Childcare	714.67m ²	25 bays
Total Parking Required:		25 bays
Total Parking Provided:		22 bays

The proposed development provides a total of 22 on-site car parking bays. This provision has a shortfall of 3 bays against the calculated requirement of 25 bays for a centre of this proposed capacity. However, considering the site’s proximity to public transport and cycling infrastructure, it is expected that the shortfall in bays will not affect operational capacity.

The adequacy of the proposed parking provision is further substantiated by an assessment of the required number of parking bays, undertaken in accordance with the requirements prescribed in WAPC Planning Bulletin 72/2009 and the WA Planning Manual (2024).

Table 9 - Car parking requirement in accordance with the WAPC Planning Bulletin 72/2009

Land Use	Requirement	Yield	Required Parking
Child Care Premises	> 1 Space per 5 Children	72 children	14.4
Total car parking requirements:			15

Table 10 - Car parking requirement in accordance with the WA Planning Manual 2024

Land Use	Requirement	Yield	Required Parking
Child Care Premises	> Minimum 0.5 space per staff member and 1 space per 20 children	72 children	> Min. 7 + 4
	> Maximum 0.5 spaces per staff member and 1 space per 5 children	13 staff	> Max. 7 + 14
Total car parking requirements:			Minimum 11 Maximum 21

An additional comparison undertaken against the WAPC Planning Bulletin 72/2009 indicates a surplus of 7 bays relative to the notional requirement of 15 bays, while the proposed provision aligns within the parking range of 11–21 bays recommended in the WA Planning Commission Planning Manual (2024).

Overall, the assessment demonstrates that while the proposed provision falls below the City of Stirling Beaufort Street LDP requirement, it exceeds or aligns with other key regional and State planning benchmarks.

Premise therefore considers that the following points further support the adequacy and suitability of the proposed car parking provision:

> Drop off function of the childcare centre

It is highly unlikely that the childcare centre would always operate at its maximum capacity. The peak time for childcare centres is typically a 2-hour period. The average length of stay, as stated in NSW RTA - Guide to Traffic Generating Developments, is 6.8 minutes. Our experience in surveying dwell times for childcare centres outside of commercial zones confirms this finding. Even assuming conservative 10 minutes average length of stay, the actual arrivals/departure rate of parents’ vehicles is likely to be spread throughout the 2-hour peak time.

The AM peak is likely to be the peak development period as most parents drop off their children before going to work, whereas the PM peak tends to be more spread out with pick up times depending on when parents become available.

The following tables were derived through many years of practice and research in this field that our office completed. We have worked with several established childcare providers who have provided sign-in data for a full week.

The percentages outlined below have emerged as the current average arrival/departure pattern. As per our transport impact assessment, the estimated average dwell time is 10 minutes, which is significantly higher than the dwell time suggested by NSW RTA Guide to Traffic Generating Developments.

While this pattern shows that up to 95% of children attend for the day (as practically recorded), the distribution still does not allow for siblings attending the centre. Furthermore, the distribution assumes that all children in attendance are driven to the childcare in a separate personal vehicle (not walked or brought on bicycles); therefore, the distribution below has a degree of conservatism.

In our previous experience, we have come across data indicating that siblings usually make up 15-25% of attendees. More than one child will be brought in a single vehicle in these cases, reducing the parking requirement.



The table below was developed on the following assumptions:

- > The arrival percentage is derived from data provided to Premise and described above.
- > It was assumed there were no siblings in the centre.
- > It was assumed that all children in attendance would be driven to the centre.

Table 11 - Changes in parking needs during the sign-in period

Sign-in Time	Extracted Arrival Percentages (of the maximum number of children)	Expected Number of Children Signing In	Parking demand (assumed dwell time 10 minutes per vehicle)
07:00 - 07:30	13.97%	10	4
07:30 - 08:30	40.55%	29	5
08:30 - 09:30	30.68%	22	4
09:30 - 10:30	7.67%	6	1
After 10:30	1.37%	1	1
Total:	94.25%	68 children (72 children – 100% capacity)	

Table 12 - Changes in parking needs during the sign-out period

Sign-Out Time	Extracted Arrival Percentages (of the maximum number of children)	Expected Number of Children Signing Out	Parking demand (assumed dwell time 10 minutes per vehicle)
Before 13:30	0.55%	1	1
13:30 - 14:30	1.92%	1	1
14:30 - 15:30	11.23%	8	2
15:30 - 16:30	24.93%	18	3
16:30 - 17:30	45.21%	33	6
17:30 - 18:30	10.41%	7	2
Total:	94.25%	68 children (72 children – 100% capacity)	

The previous tables show that the parking demand is the strongest in the periods 07.30 - 08:30 and 16:30-17:30. When applied to the subject development, with an assumed dwell time of 10 minutes per vehicle, the childcare centre would require a maximum of 5 car bays to cater for the expected parking demand for the drop off and 6 car bays for pick-up function.

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> **Staff arrival and presence on site**

Based on our prior experience with similar projects and the available data on staff arrival and attendance patterns, Premise anticipates that 13 staff members of the subject development will work in shifts, as shown in the following table:

Table 13 - Staff arrival and attendance patterns

	06:30	07:00	07:30	08:00	08:30	09:00	09:30	10:00	10:30	11:00	11:30	12:00	12:30	13:00	13:30	14:00	14:30	15:00	15:30	16:00	16:30	17:00	17:30	18:00
ES	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2	0	0	0	0	0	0
MS	0	0	0	0	4	4	4	4	4	4	4	4	4	4	4	4	4	4	4	4	4	4	0	0
LS	0	0	0	0	0	0	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2
SSL	0	0	0	0	0	0	0	4	4	4	4	4	4	4	4	4	4	4	0	0	0	0	0	0
SS	1	1	1	1	1	1	1	0	0	0	0	0	0	0	0	0	0	0	1	1	1	1	1	1
STAFF ON-SITE	3	3	3	3	7	7	9	12	12	12	12	12	12	12	12	12	12	12	7	7	7	7	3	3

Note – Abbreviations in the table above:
 · ES- Early shift 6:30am-3pm
 · MS - Middle shift 8:30am-5pm

· LS - Late shift 9:30am-6pm
 · SSL - Short shift for Lunch cover 10am-3pm
 · SS - split shift 6:30am-9:30am`

Therefore, it can be concluded that up to 13 staff members may be present on-site at any given time. However, during peak drop-off and pick-up periods (07:00-09:00 and 16:00-18:00), a maximum of 7 staff members would be on the premises.

During the middle of the day (outside of drop off and pick up times), when the majority of staff are expected to be on-site, staff will be able to use some of the visitors' bays, with plenty of spare capacity left. Furthermore, it is anticipated that some staff members might cycle or walk to the development site, given the walkable surrounding network. Given the public transport links, the staff should be encouraged to use public transport as much as possible. Additionally, it is not uncommon for the childcare centres to take on junior staff that is below driving age, therefore not inducing parking demand for prolonged period.

> **Expected on-site car parking requirement throughout the operational hours of the subject development**

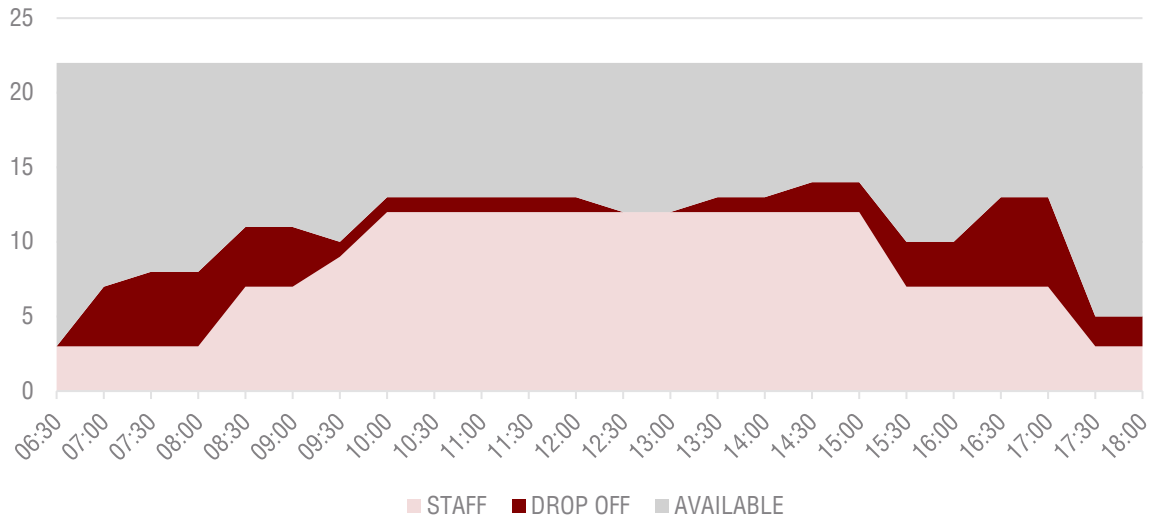
The following table and graph clearly demonstrate the expected on-site car parking requirement throughout the full operational hours of the childcare facility.



Table 14 - Expected on-site car parking requirement throughout the full operational hours

	06:30	07:00	07:30	08:00	08:30	09:00	09:30	10:00	10:30	11:00	11:30	12:00	12:30	13:00	13:30	14:00	14:30	15:00	15:30	16:00	16:30	17:00	17:30	18:00
STAFF	3	3	3	3	7	7	9	12	12	12	12	12	12	12	12	12	12	12	7	7	7	7	3	3
DROP OFF	0	4	5	5	4	4	1	1	1	1	1	1	0	0	1	1	2	2	3	3	6	6	2	2
Expected car parking demand during operational time																								
OCCUPIED	3	7	8	8	11	11	10	13	13	13	13	13	12	12	13	13	14	14	10	10	13	13	5	5
Available on-site car parking provision																								
AVAILABLE	19	15	14	14	11	11	12	9	9	9	9	9	10	10	9	9	8	8	12	12	9	9	17	17

Figure 9 - Parking Demand Graph



Based on the detailed assessment of applicable parking requirements, operational characteristics and the specific site context, it is concluded that the proposed 22 on-site car parking bays will be sufficient to meet the anticipated parking demand of the proposed childcare centre.

Furthermore, the staff demand conservatively assumes that all staff will arrive in their own personal vehicles and will require a parking spot for the duration of their shift. Given the location of the proposed development, it is reasonable to assume that staff will take advantage of convenient public transport connections and high active transport accessibility.



Accordingly, Premise concludes that the proposed car parking provision is appropriate and sufficient to support the day-to-day operation of the childcare centre and aligns with broader State and local planning objectives promoting accessibility, sustainability and integrated transport outcomes.

4.2.1 OVERVIEW OF COMPLIANCE WITH AS2890 PARKING FACILITIES

The proposed development should adhere to the Australian/New Zealand Standard for parking facilities (AS 2890.01), which prescribes geometric and design requirements for off-street car parking facilities; and Part 6: Off-street parking for people with disabilities – AS2890.06.

The site will provide 22 parking bays.

Parking areas are designed to accommodate User Class 1 - Employee and commuter parking (generally, all-day parking), User Class 3A - Short term, high turnover parking at shopping centres and User Class 4 - Parking for people with disabilities

The access driveway is classified as Category 1, allowing direct access to the parking bays from Ingleton Lane.

4.2.1.1 Comparison of proposed layout to AS2890.01 requirements

Table 15 - Parking dimensions comparisons

Parking Bay Type	AS2890.1:2004 Off-street car parking AS2890.6 Off-street parking for people with disabilities					
	Parking Bay Length		Parking Bay Width		Aisle Width	
	Required	Proposed	Required	Proposed	Required	Proposed
All bays at 90° (User Class 1) RESIDENTS	5.4m	5.5m / 5.04m* (sufficient with overhang)	2.4m	2.5m	5.8m	6.1m
All bays at 90° (User Class 3) VISITORS	5.4m	5.5m / 5.04m* (sufficient with overhang)	2.6m	2.5m	5.8m	6.1m
ACROD Parking	5.4m	5.5m	2.4m–ACROD 2.4m–shared space	2.5m 2.5m	5.8m	6.1m

*Note * - where parking is controlled by wheelstops installed at right angles to the direction of parking such is the case with parking bays on one side of the parking aisle, the minimum car parking length is 5.4m with wheel stop positioned in line with the AS2890.1:2004*



Table 16 – Parking design and layout comparison

REQUIREMENT	COMPLIANCE
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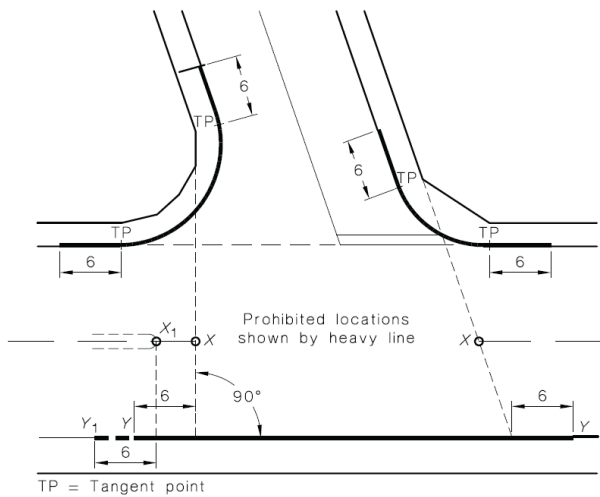
“3.2.3 Access driveway location

(a) Driveway Categories 1 and 2

At unsignalized intersections of sub-arterial, collector or local streets with each other or with an arterial road, access driveways in Categories 1 and 2 (see Table 3.1) shall not be located in the sections of kerb shown by heavy lines in Figure 3.1. This requirement shall not apply to accesses to domestic driveways in the kerb section opposite the entering road at any intersection including signalized intersections. Furthermore, it shall not apply to any access driveway serving a property which would otherwise be denied access due to the physical impossibility of meeting the requirement.”

- The proposed driveway is located approximately 30m from the intersection of Ingleton Lane and Second Avenue which is in accordance with the standard.

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“3.2.1 Access driveway widths

**TABLE 3.1
 SELECTION OF ACCESS FACILITY CATEGORY**

Class of parking facility (see Table 1.1)	Frontage road type	Access facility category				
		Number of parking spaces (Note 1)				
		<25	25 to 100	101 to 300	301 to 600	>600
1,1A	Arterial	1	2	3	4	5
	Local	1	1	2	3	4
2	Arterial	2	2	3	4	5
	Local	1	2	3	4	4
3,3A	Arterial	2	3	4	4	5
	Local	1	2	3	4	4

NOTES:
 1 When a car park has multiple access points, each access should be designed for the number of parking spaces effectively served by that access.
 2 This Table does not imply that certain types of development are necessarily suitable for location on any particular frontage road type. In particular, access to arterial roads should be limited as far as practicable, and in some circumstances it may be preferable to allow left-turn-only movements into and out of the access driveway.

**TABLE 3.2
 ACCESS DRIVEWAY WIDTHS**

Category	Entry width	Exit width	Separation of driveways
1	3.0 to 5.5	(Combined) (see Note)	N/A
2	6.0 to 9.0	(Combined) (see Note)	N/A
3	6.0	4.0 to 6.0	1 to 3
4	6.0 to 8.0	6.0 to 8.0	1 to 3
5	To be provided as an intersection, not an access driveway, see Clause 3.1.1.		

NOTE: Driveways are normally combined, but if separate, both entry and exit widths should be 3.0 m min.

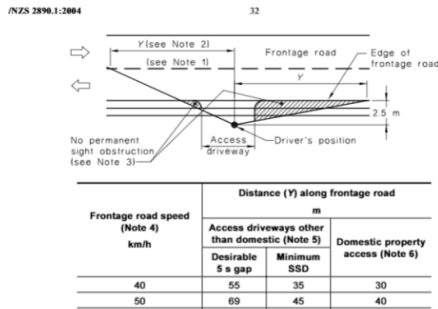
- The proposed crossover is classified as a Category 1 driveway, which typically requires a combined width of 3.0 to 5.5m. The actual width provided is 6.1m, which exceeds the standard. However, this provides additional safety and operational flexibility for vehicles entering and exiting the site.

REQUIREMENT	COMPLIANCE
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3.2.4 Sight distance at access driveway exits

(b) Sight distance to pedestrians

Clear sight lines as shown in Figure 3.3 shall be provided at the property line to ensure adequate visibility between vehicles leaving the car park or domestic driveway and pedestrians on the frontage road footpath."



- Fire stairs and bin pads partially blocking sight distances to the laneway from the access point

2.4.2 Angle parking aisle

(c) Blind aisles

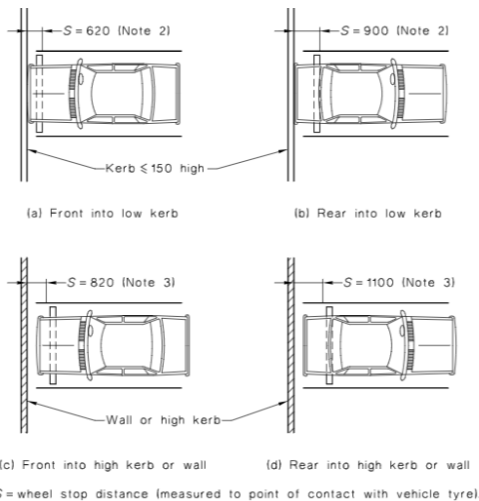
At blind aisles, the aisle shall be extended a minimum of 1 m beyond the last parking space, as shown in Figure 2.3, and the last parking space widened by at least 300 mm if it is bounded by a wall or fence.

- There is a dedicated reversing bay proposed
- Blind aisles is extended by more than 1m beyond the last parking space
- Parking bays bounded by a wall or object are widened by at least 300mm

2.4.5.4 WHEEL STOPS

Wheel stops shall be between 90 and 100 mm in height, and 1650±50mm in width.

If wheel stops are provided to restrain vehicle contact with a kerb higher than 150mm or a wall, a further 200mm shall be added to the wheel stop distance to cater for the B99 vehicle as illustrated in Figure 2.6(c) and (d).



- There are wheel stops proposed to allow vehicle overhang in bays with insufficient length. However, the wheel stops need to be positioned in accordance with the requirements set out in AS 2890.1.

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4.2.2 OVERVIEW OF COMPLIANCE WITH THE CITY OF STIRLING’S CROSSOVER GUIDELINES RESIDENTIAL AND NON-RESIDENTIAL

All vehicular access to the site will be provided via a single crossover from Ingleton Lane with a proposed width of 6.1 metres. This complies with the City of Stirling’s specifications, which stipulate a minimum width of 6.0 metres and a maximum of 10.0 metres.

4.2.3 VEHICLE SWEEP PATHS

The proposed parking area has been tested using a B99 Passenger Vehicle (5.2m). The reversing bay requires widening and/or the proposed kerb should be kept low to allow for vehicle chassis overhang while manoeuvrings. This matter can be readily addressed in subsequent design phases. Apart from this, no navigability issues were identified. For further details, please refer to the swept path analysis plans in Appendix C.

4.3 ACROD Parking

Table 17 - Accessible car parking provision rates

Guideline document	Building class	Car parking provision
NCC 2022 Building Code of Australia - Volume One	<ul style="list-style-type: none"> Class 9b — (b) Other assembly building — (i) up to 1000 carparking spaces; - 1 space for every 50 carparking spaces or part thereof 	1
Total Required		1
Total Provided		1

The requirements for accessible parking size for this type of development is 1 bay. The proposed development has provided 1 accessible bay.

4.4 Bicycle Parking

The Beaufort Street LDP states that bicycle parking shall be provided in accordance with the City’s Local Planning Policy 6.2 Bicycle Parking. However, as LPP 6.2 is unavailable, the provisions set by LPP 6.7 are used.

Table 18 – Bicycle Parking calculations

Guideline document	Bicycle parking requirements	Bike parking provision
City of Stirling’s LPP 6.7 – Parking and Access	<ul style="list-style-type: none"> All other uses: 1 space per 400m² of GFA 	2 bays
Total Required		2
Total Provided		4

The proposal includes the provision of 4 dedicated bicycle parking bays for use by staff or visitors, which is a surplus to requirements. This provision is considered more than sufficient to cater to any potential demand.



4.5 Delivery and Service

All refuse and recycling materials for the proposed development will be managed from a dedicated bin store near the access point of the site on Ingleton Lane.

No service vehicle access or loading facilities are proposed along the Beaufort Street frontage. All servicing activity has been internalised to the rear of the site to preserve the pedestrian amenity and activation of the primary activity corridor.

Service vehicles will access the site via the Ingleton Lane laneway. As noted in section 6.1.5 of the LDP, the laneway is being widened to a width of 6m. This increased width allows for entry and exits of service and commercial vehicles in front gear.

4.6 Traffic Impact of the Proposed Development

Data on the trip-generating potential of the various land uses is fairly limited in Western Australia including for childcare premises. The NSW Guide to Transport Impact Assessment (GTIA) was updated and published in 2024, after extensive engagement with industry professionals, therefore these new rates will be used.

Table 19 - Trip generation rates

Guideline document	Trip generation rates
NSW Guide to Transport Impact Assessment (GTIA)	<ul style="list-style-type: none"> • Site AM peak hour = 0.81 vehicle trips/licensed child places • Site PM peak hour = 0.80 vehicle trips/licensed child places • Daily = 2.97 vehicle trips/licensed child places

Table 20 - Calculation of vehicular trips

Land Use Type	Yield	Daily Traffic Generation	Peak Hour Traffic Generation	
			AM	PM
Childcare Centre	72 child places	214	58	58

According to WAPC guidelines, developments generating between 10-100 vehicular trips in the peak hours can be considered to have a moderate impact on the road network.

The proposed development is expected to generate 214 vehicular trips daily, including 58 vehicular trips in the AM peak and 58 vehicular trips in the PM peak. The surrounding road network is expected to successfully absorb the additional traffic.

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4.7 Trip Distribution

The total projected traffic movements to and from the site have been based on the surrounding land uses, anticipated desire lines and the nature of the proposed development. These estimates consider peak hour flows, nearby intersections and likely distribution patterns. Estimated traffic distribution percentages are outlined in the table below for reference.

Table 15 outlines the anticipated traffic distribution based on the above description.

Table 21 - Trip Distribution Routes

Route	Percentage
> From the north via Beaufort Street >> Second Avenue >> Ingleton Lane >> subject development and reverse	15%
From the northwest via Second Avenue >> Ingleton Lane >> subject development and reverse	10%
> From the southeast via Beaufort Street >> First Avenue >> Ingleton Lane >> subject development and reverse	65%
> From the south via First Avenue >> Ingleton Lane >> subject development and reverse	10%

4.8 Site-Specific Issues and Proposed Remedial Measures

The assessment of the additional traffic generated by the proposed development, concluded the following:

Table 22 – Analysed Site-Specific Issues and Proposed Remedial Measures

How many site-specific issues need to be discussed?	
Site-Specific Issue No 1	> Parking provision
Remedial Measure / Response	Premise believes that the actual parking demand for the proposed childcare centre is lower than the anticipated requirement. The provision of 22 on-site car parking bays is considered sufficient to accommodate the parking demand of the subject development. This is elaborated in detail in Section 4.2 within this report.



APPENDICES

APPENDIX A

DEVELOPMENT SITE PLAN

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APPENDIX B

TRANSPORT PLANNING AND TRAFFIC PLANS

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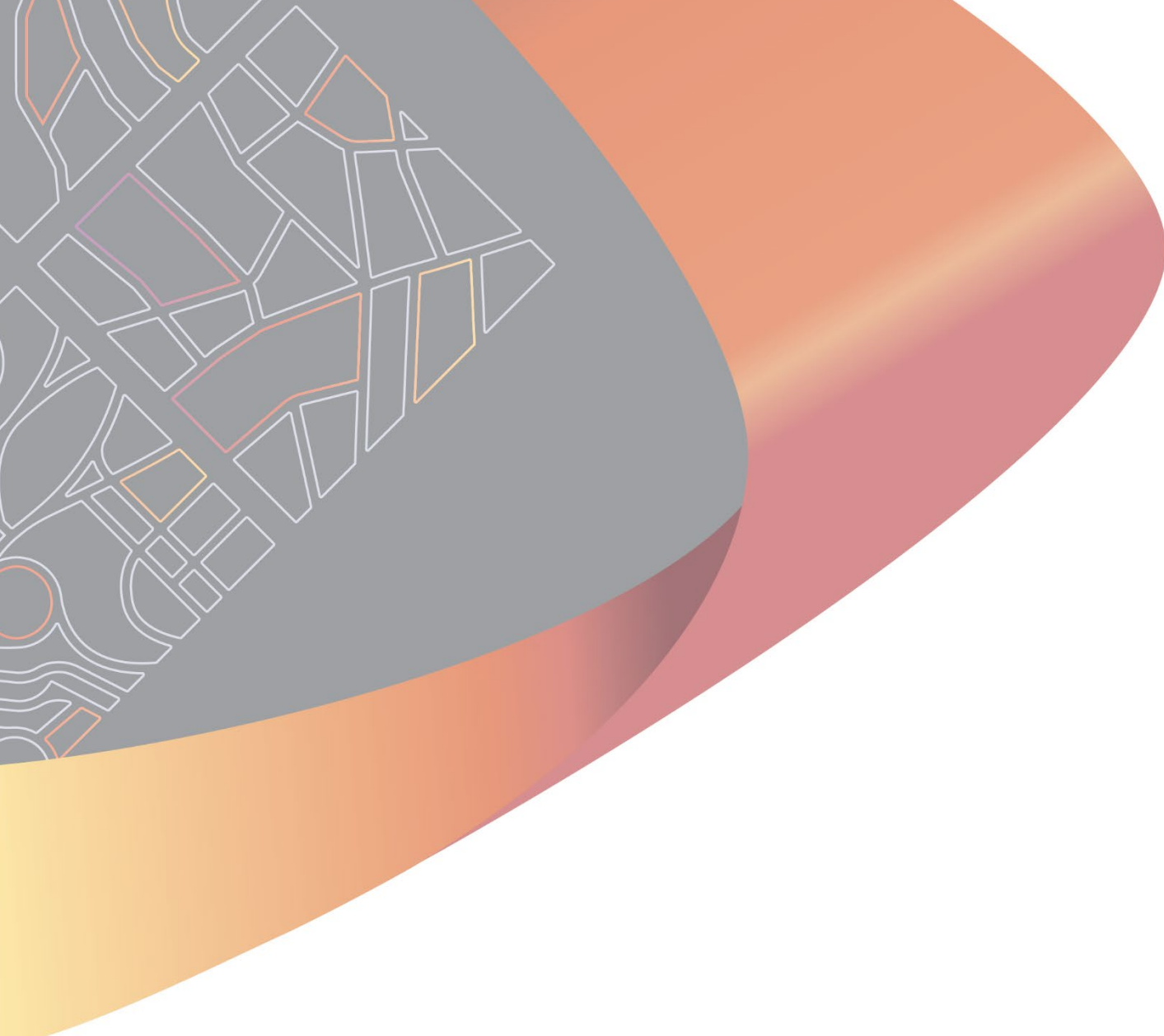


APPENDIX C

VEHICLE SWEEP PATH ANALYSIS

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Address: Lot 112 (#752) Beaufort St, Mt Lawley

Childcare Centre

Job Number: 22082

Drawing No	Description
01	Cover Page
02	3D Street Perspective
03	3D Perspectives
04	Existing Site Survey
05	Site Plan
06	Context Plan
07	Overlay
08	Ground Floor Plan
09	First Floor Plan
10	Elevations
11	Elevations
12	Roof Plan
13	Section & Solar Study
14	Breeze Path Plan

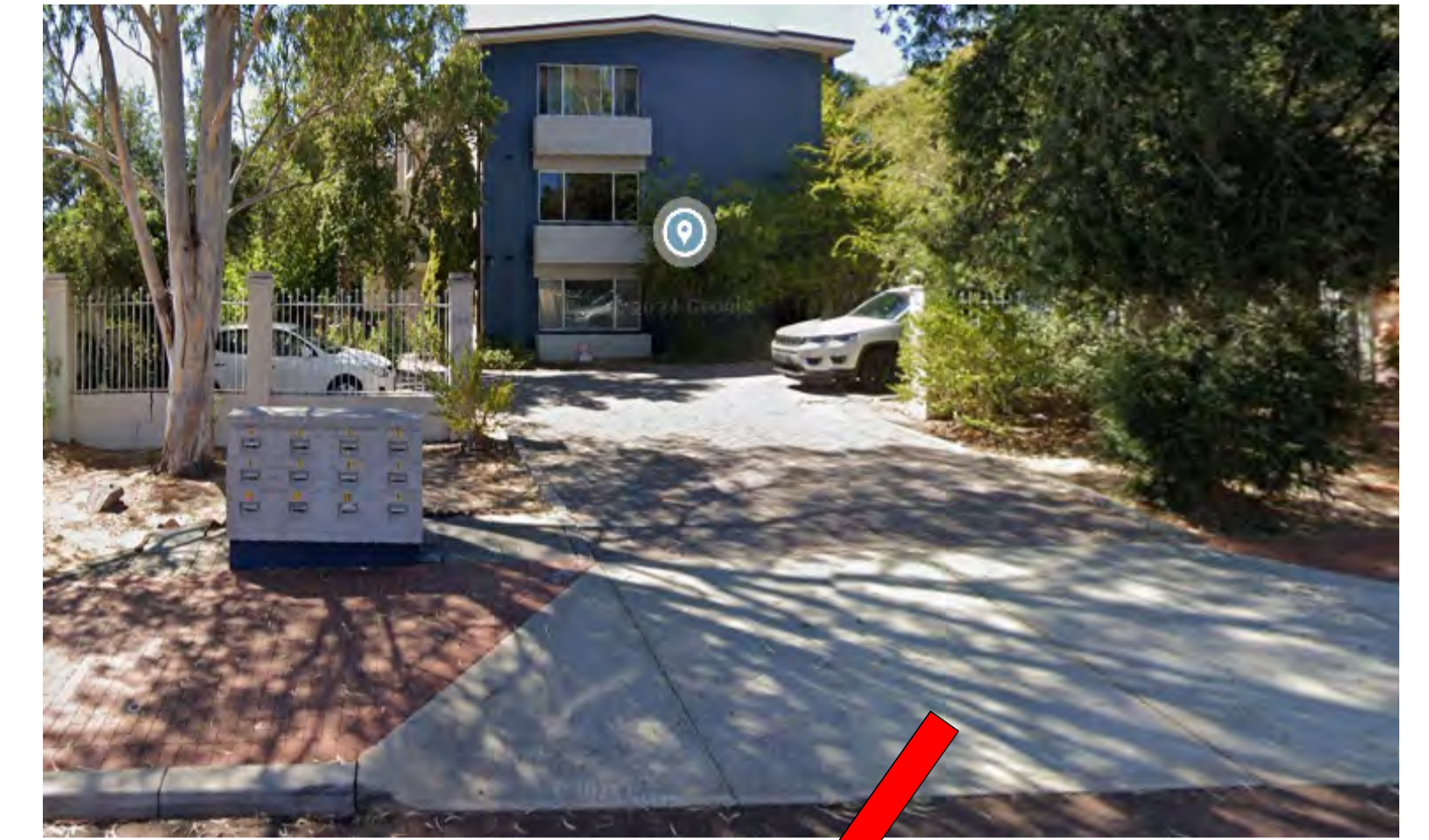
City of Stirling
1 Apr 2026
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760 BEAUFORT STREET, MOUNT LAWLEY



746 BEAUFORT STREET, MOUNT LAWLEY



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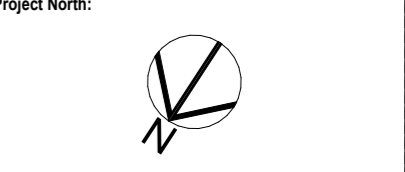
Revision	Description	Date
6.00	Development Approval	04.02.26
5.00	Heritage Comments	17.10.25
4.00	Heritage Comments	02.08.25
3.00	DRP	10.04.25
2.00	Changes	04.04.25
1.00	Concept	26.02.25

Project Name:
Childcare Centre
Project Address:
Lot 112 (#752) Beaufort St, Mt Lawley
Drawing Title:
3D Street Perspective
Scale:
1:3.33
Project No:
22082
Drawing No.:
02 of 14

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Revision	Description	Date
6.00	Development Approval	04.02.26
5.00	Heritage Comments	17.10.25
4.00	Heritage Comments	02.08.25
3.00	DBP	10.04.25
2.00	Changes	04.04.25
1.00	Concept	08.02.25



Project Name
Childcare Centre
Project Address
Lot 112 (#752) Beaufort St, Mt Lawley

Drawing Title:
3D Perspectives

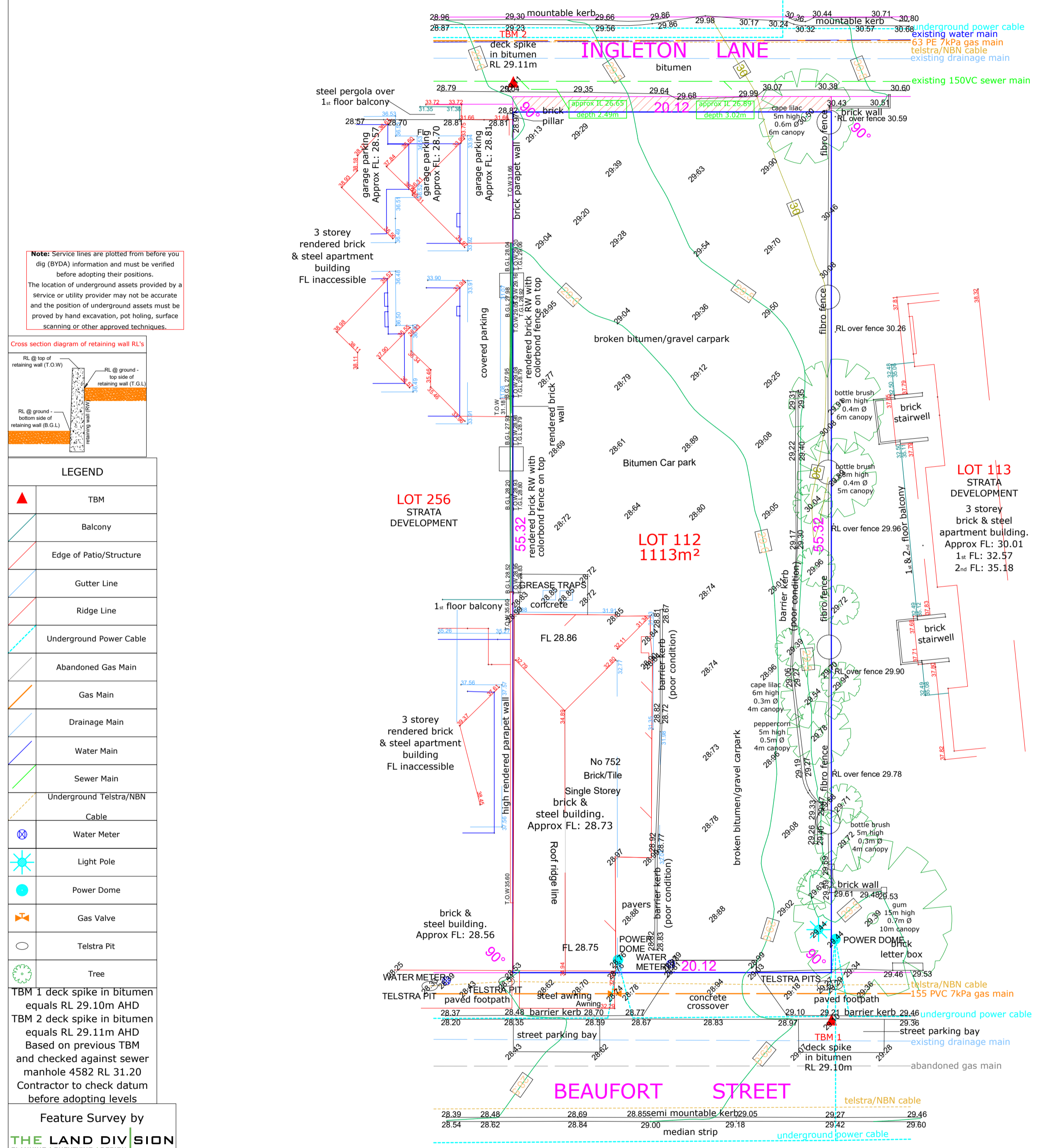
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Project No: 22082 Revision Number: 6.00

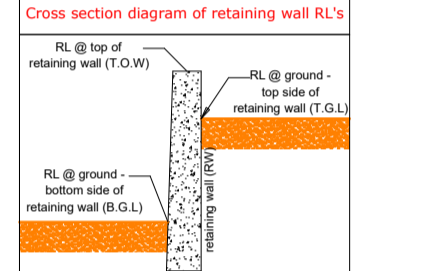
Drawing No.: 03 of 14

NOTE: No title viewed by The Land Division. A certificate of title check for easements and encumbrances is highly recommended as should they exist, they may affect design.

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1 Apr 2026
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Note: Service lines are plotted from before you dig (BYDA) information and must be verified before adopting their positions. The location of underground assets provided by a service or utility provider may not be accurate and the position of underground assets must be proved by hand excavation, pot holing, surface scanning or other approved techniques.



LEGEND	
	TBM
	Balcony
	Edge of Patio/Structure
	Gutter Line
	Ridge Line
	Underground Power Cable
	Abandoned Gas Main
	Gas Main
	Drainage Main
	Water Main
	Sewer Main
	Underground Telstra/NBN Cable
	Water Meter
	Light Pole
	Power Dome
	Gas Valve
	Telstra Pit
	Tree

TBM 1 deck spike in bitumen equals RL 29.10m AHD
TBM 2 deck spike in bitumen equals RL 29.11m AHD
Based on previous TBM and checked against sewer manhole 4582 RL 31.20
Contractor to check datum before adopting levels

Feature Survey by
THE LAND DIVISION
PLANNING & SURVEYING (12888)
PO Box 2444,
Malaga, WA 6090
phone: 08 9209 3232
www.landdivision.com.au

Survey Date: 22 September 2025 Scale 1:200@A3
Client: Germano Designs

Rev	Date	Description	Surv	Drawn
0	24/09/2025	Feature Survey Drafted	TF	TF

FEATURE AND CONTOUR SURVEY OF LOT 112 ON PLAN 2473
752 Beaufort Street, Mount Lawley
C/T Vol: 1485 Fol: 145
our ref. 25-0860

NOTES: 1) CONSULT LEGAL ADVICE ON EASEMENTS, ENCUMBRANCES AND CAVEATS THAT MAY APPEAR ON THE CERTIFICATE OF TITLE. 2) LEVELS ON ADJOINING PROPERTIES ARE APPROXIMATE DUE TO ACCESS RESTRICTIONS. 3) SERVICES PLOTTED AS VISUALLY SEEN ON SITE AND ARE APPROXIMATE. 4) SEWER POSITION AND LEVELS FROM WATER CORPORATION PLANS. 5) CONSULT BEFORE YOU DIG (BYDA) TO CHECK LOCATION OF UNDERGROUND SERVICES. 6) BEWARE OF OVERHEAD POWER LINE HAZARDS. 7) CONSULT TLD ON ANY ANOMALY BEFORE DESIGN AND CONSTRUCTION. 8) POSITION AND DEPTH OF SERVICES TO BE CONFIRMED ON SITE BY CONTRACTOR. 9) FEATURES ARE RELATED TO FENCE LINES ONLY. NO CONNECTION MADE TO BOUNDARIES. REPEP RECOMMENDED.



Revision/Description	Date
6.00 Development Approval	04.02.26
5.00 Heritage Comments	17.10.25
4.00 Heritage Comments	02.06.25
3.00 DRP	10.04.25
2.00 Changes	04.04.25
1.00 Concept	26.02.25



Project Name
Childcare Centre
Project Address
Lot 112 (#752) Beaufort St, Mt Lawley
Drawing Title:
Existing Site Survey

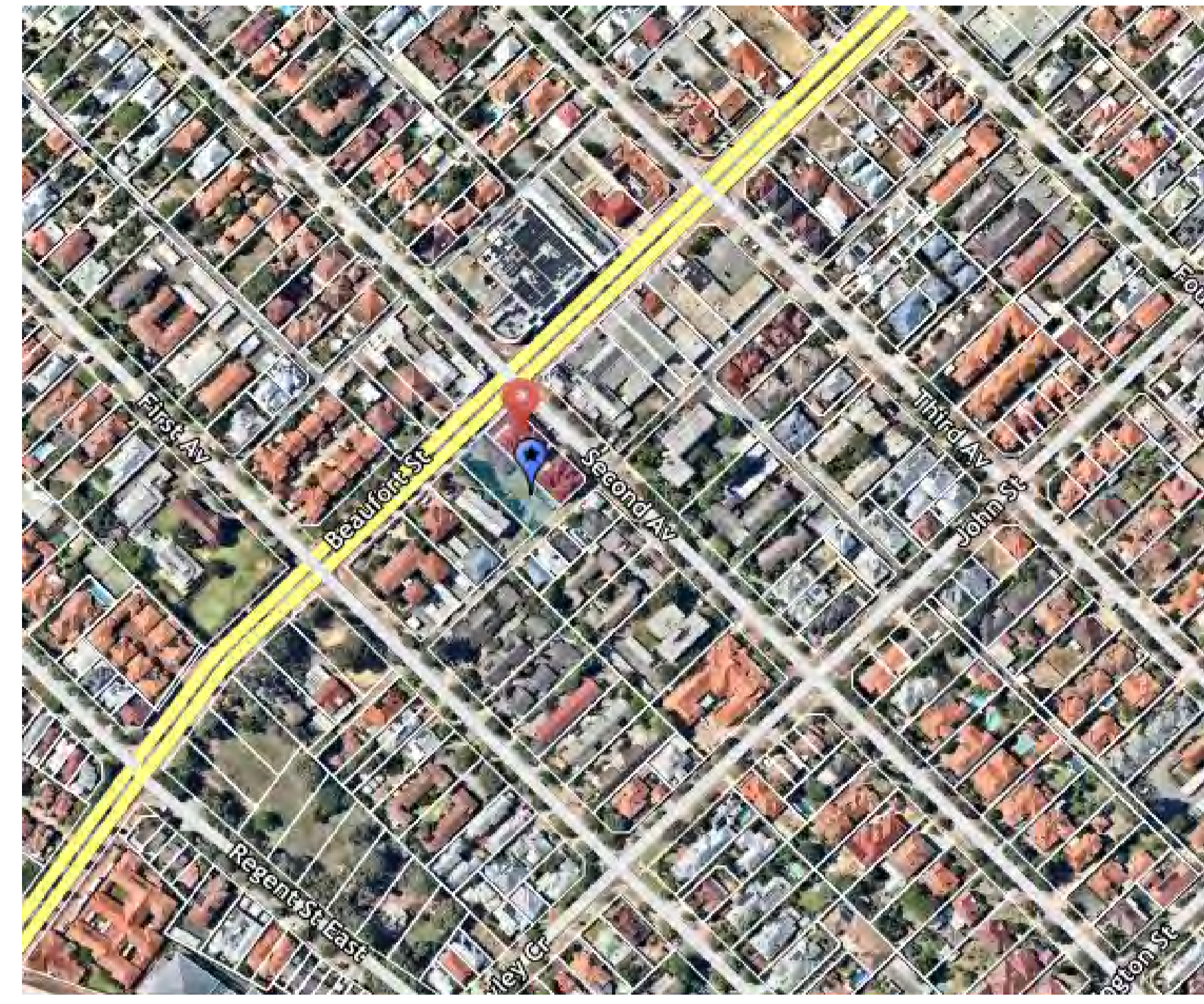
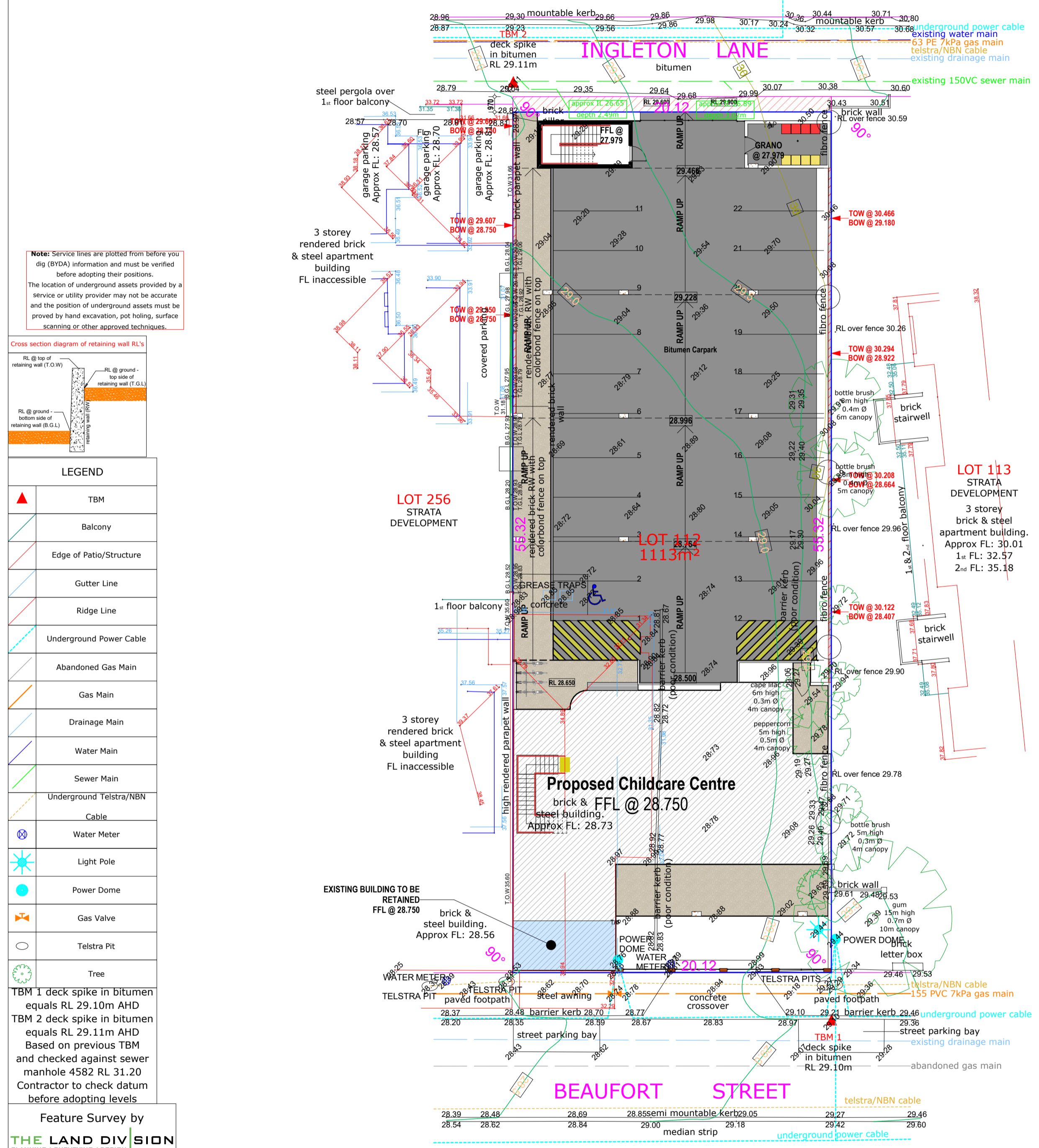
Scale:	Sheet Size:
1:200	A1
Project No:	Revision Number:
22082	6.00

Drawing No.:
04 of 14

Unit: 311 Mulgill Road, Malaga WA 6090
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1 Apr 2026
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LEGEND

- TBM
- Balcony
- Edge of Patio/Structure
- Gutter Line
- Ridge Line
- Underground Power Cable
- Abandoned Gas Main
- Gas Main
- Drainage Main
- Water Main
- Sewer Main
- Underground Telstra/NBN Cable
- Water Meter
- Light Pole
- Power Dome
- Gas Valve
- Telstra Pit
- Tree

TBM 1 deck spike in bitumen equals RL 29.10m AHD
TBM 2 deck spike in bitumen equals RL 29.11m AHD
Based on previous TBM and checked against sewer manhole 4582 RL 31.20
Contractor to check datum before adopting levels

Feature Survey by
THE LAND DIVISION
PLANNING & SURVEYING | DESIGN
PO Box 2444,
Malaga, WA 6090
phone: 08 9209 3232
www.landdivision.com.au

Survey Date: 22 September 2025 | Scale 1:200@A3
Client: Germano Designs

Rev	Date	Description	Surv	Drawn
0	24/09/2025	Feature Survey Drafted	TF	TF

veranda

FEATURE AND CONTOUR SURVEY OF LOT 112 ON PLAN 2473
752 Beaufort Street, Mount Lawley
C/T Vol: 1485 Fol: 145
our ref. 25-0860

NOTES: 1) CONSULT LEGAL ADVICE ON EASEMENTS, ENCUMBRANCES AND CAVEATS THAT MAY APPEAR ON THE CERTIFICATE OF TITLE. 2) LEVELS ON ADJOINING PROPERTIES ARE APPROXIMATE DUE TO ACCESS RESTRICTIONS. 3) SERVICES PLOTTED AS VISUALLY SEEN ON SITE AND ARE APPROXIMATE. 4) SEWER POSITION AND LEVELS FROM WATER CORPORATION PLANS. 5) CONSULT BEFORE YOU DIG (BYDA) TO CHECK LOCATION OF UNDERGROUND SERVICES. 6) BEWARE OF OVERHEAD POWER LINE HAZARDS. 7) CONSULT TLD ON ANY ANOMALY BEFORE DESIGN AND CONSTRUCTION. 8) POSITION AND DEPTH OF SERVICES TO BE CONFIRMED ON SITE BY CONTRACTOR. 9) FEATURES ARE RELATED TO FENCE LINES ONLY. NO CONNECTION MADE TO BOUNDARIES. REPEP RECOMMENDED.

Zone	Area	Perim
First Floor		
Fire Stairs	17.94	17.980
Chickare	482.28	120.062
	482.22 m ²	138.042 m
Ground Floor		
Services	10.80	13.900
Site Base	16.15	16.780
Fire Stairs	17.94	17.980
Chickare	252.39	76.089
	297.28 m ²	124.649 m
	777.50 m ²	262.891 m

Site Calculations

Site Area: 1,113m²
Building Footprint: 270.32m²
Site Coverage: 24.28%
Allowable Site Coverage: NA%

Zoning: NA
Policies: Mixed Use
Heritage: NA
Bushfire: NA
BAL: NA
Acoustic: TBC
Sewer: Ingleton Lane
Power: Above
Coastal: NA
Water: TBC
Wind Rating: TBC

Revision	Description	Date
6.00	Development Approval	04.02.26
5.00	Heritage Comments	17.10.25
4.00	Heritage Comments	02.08.25
3.00	DRP	10.04.25
2.00	Changes	04.04.25
1.00	Concept	28.02.25

Project North:

Project Name
Childcare Centre

Project Address
Lot 112 (#752) Beaufort St, Mt Lawley

Drawing Title
Site Plan

Scale: 1:200 | Sheet Size: A1
Project No: 22082 | Revision Number: 6.00

Drawing No.: 05 of 14

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66 Second Ave, Mount Lawley 6050



760 Beaufort St, Mount Lawley 6050



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1 Apr 2026
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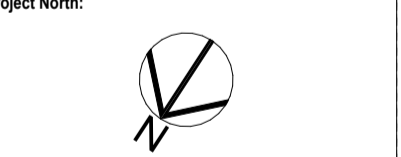


31 Lawley Cres, Mount Lawley 6050



755 Beaufort St, Mount Lawley 6050

Revision	Description	Date
6.00	Development Approval	04.02.26
5.00	Heritage Comments	17.10.25
4.00	Heritage Comments	02.06.25
3.00	DRP	10.04.25
2.00	Changes	04.04.25
1.00	Concept	28.02.25



Project Name
Childcare Centre
Project Address
Lot 112 (#752) Beaufort St, Mt Lawley
Drawing Title
Context Plan

Scale:	Sheet Size:	A1
Project No:	Revision Number:	22082 6.00
Drawing No.:	06 of 14	



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Revision	Description	Date
6.00	Development Approval	04.02.26
5.00	Heritage Comments	17.10.25
4.00	Heritage Comments	02.06.25
3.00	DRP	10.04.25
2.00	Changes	04.04.25
1.00	Concept	26.02.25



Project Name
Childcare Centre
Project Address
Lot 112 (#752) Beaufort St, Mt Lawley
Drawing Title
Overlay

Scale: Sheet Size: A1

Project No: 22082 Revision Number: 6.00

Drawing No.: 07 of 14



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Childcare Calculations

Room	Age (Yrs)	Quant.	Size	Staff Req
Activity 1	0-2	12	41.49m ²	3
Activity 2	2-3	10	40.29m ²	2
Activity 3	2-3	10	34.68m ²	2
Activity 4	3+	20	65.78m ²	2
Activity 5	3+	20	43.07m ²	2
Atelier			22.62m ²	
Piazza			43.07m ²	
				2

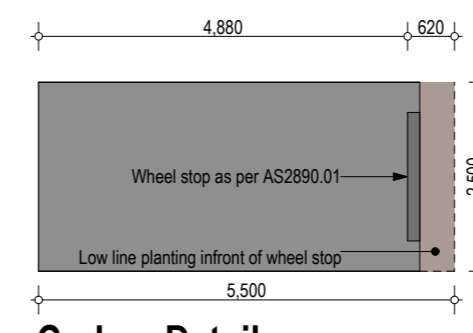
Total Internal = 72 **291.007m² 13**
(Min 3.25m² per child) (Min 234.00m² req)

Total External Play Area = 72 **515.06.86m²**
(Min 7m² per child) (Min 504.00m² req)

City of Stirling Parking Calculations

Description	Req	Provided
1 bay per 7 childs and 1 per staff		
Child Bays	11	8
Staff Bays	13	13
	24	21

City of Stirling
1 Apr 2026
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Carbay Detail



Zone	Area	Perim
First Floor		
Fire Stairs	17.94	17.980
Childcare	482.28	120.062
	482.22 m ²	138.042 mm
Ground Floor		
Services	10.80	13.800
Bin Store	16.15	16.780
Fire Stairs	17.94	17.980
Childcare	352.39	76.989
	297.28 m ²	124.649 mm
	777.58 m ²	262.881 mm

Site Calculations	
Site Area:	1,113m ²
Building Footprint:	270.33m ²
Site Coverage:	24.26%
Allowable Site Coverage:	N/A%
Zoning:	NA
Police:	Mixed Use
Heritage:	NA
Bushfire:	NA
BAL:	NA
Acoustic:	TBC
Sever:	Ingleton Lane
Power:	Above
Coastal:	NA
Water:	TBC
Wind Rating:	TBC

Revision	Description	Date
6.00	Development Approval	04.02.26
5.00	Heritage Comments	17.10.25
4.00	Heritage Comments	12.08.25
3.00	DRP	10.04.25
2.00	Changes	04.04.25
1.00	Concept	28.02.25

Project Name:	Childcare Centre
Project Address:	Lot 112 (#752) Beaufort St, Mt Lawley
Drawing Title:	Ground Floor Plan
Scale:	1:100
Sheet Size:	A1
Project No:	22082
Revision Number:	6.00
Drawing No.:	08 of 14

GERMANO DESIGNS

Unit: 3/1 Mulgill Road, Malaga WA 6009
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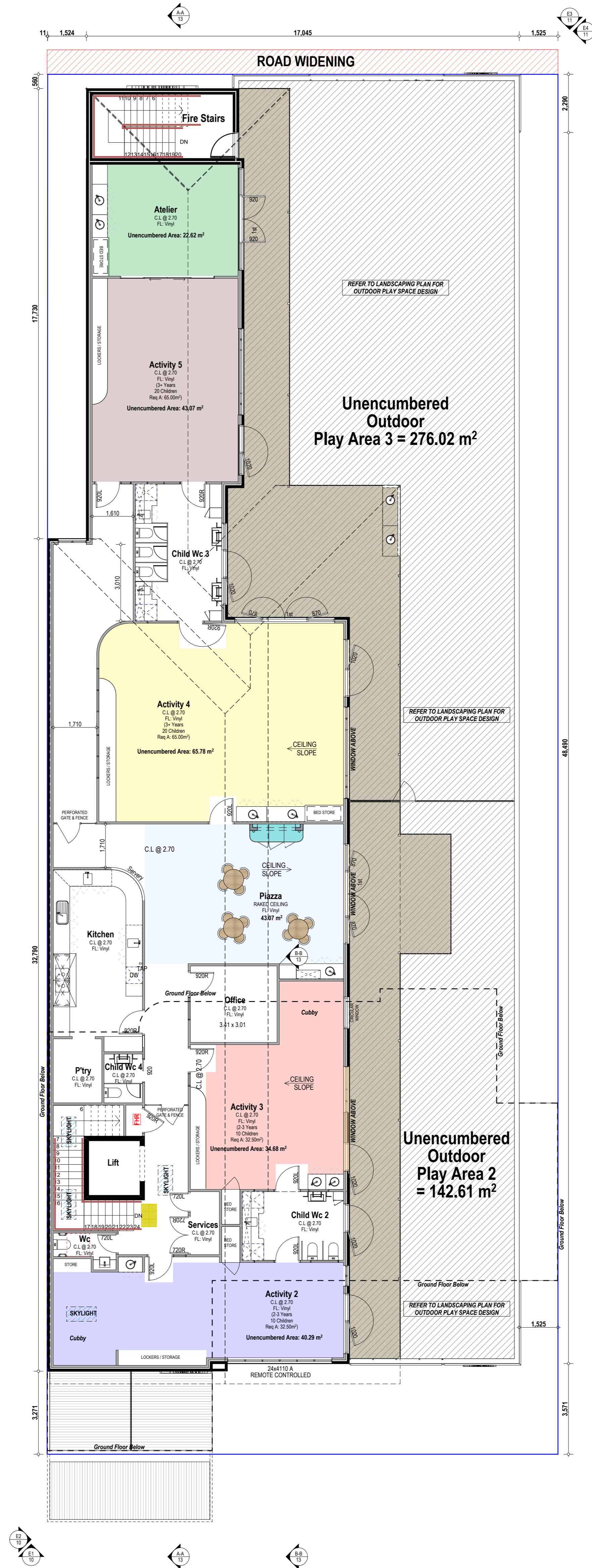
Childcare Calculations

Room	Age (Yrs)	Quant.	Size	Staff Req
Activity 1	0-2	12	41.49m ²	3
Activity 2	2-3	10	40.29m ²	2
Activity 3	2-3	10	34.68m ²	2
Activity 4	3+	20	65.78m ²	2
Activity 5	3+	20	43.07m ²	2
Atelier			22.62m ²	
Piazza			43.07m ²	
				2

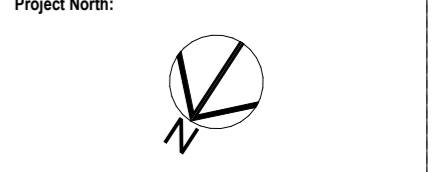
Total Internal = 72 **291.007m² 13**
(Min 3.25m² per child) (Min 234.00m² req)

Total External Play Area = 72 **515.06.86m²**
(Min 7m² per child) (Min 504.00m² req)

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Revision	Description	Date
6.00	Development Approval	04.02.26
5.00	Heritage Comments	17.10.25
4.00	Heritage Comments	02.08.25
3.00	DRP	10.04.25
2.00	Changes	04.04.25
1.00	Concept	05.02.25



Project Name
Childcare Centre
Project Address
Lot 112 (#752) Beaufort St, Mt Lawley

Drawing Title
First Floor Plan

Scale: 1:100 Sheet Size: A1

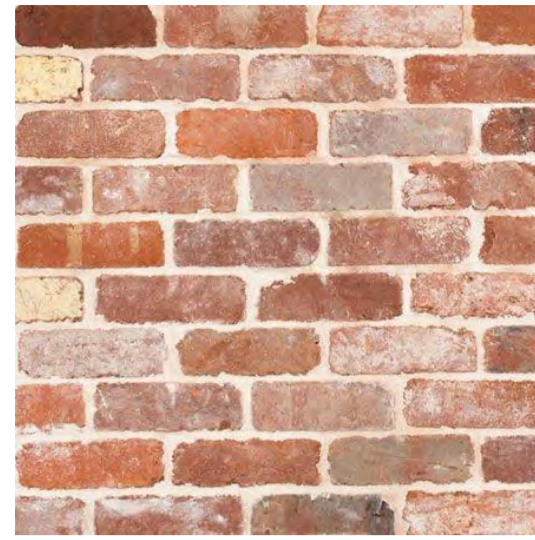
Project No: 22082 Revision Number: 6.00

Drawing No.: 09 of 14

G
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DESIGNS

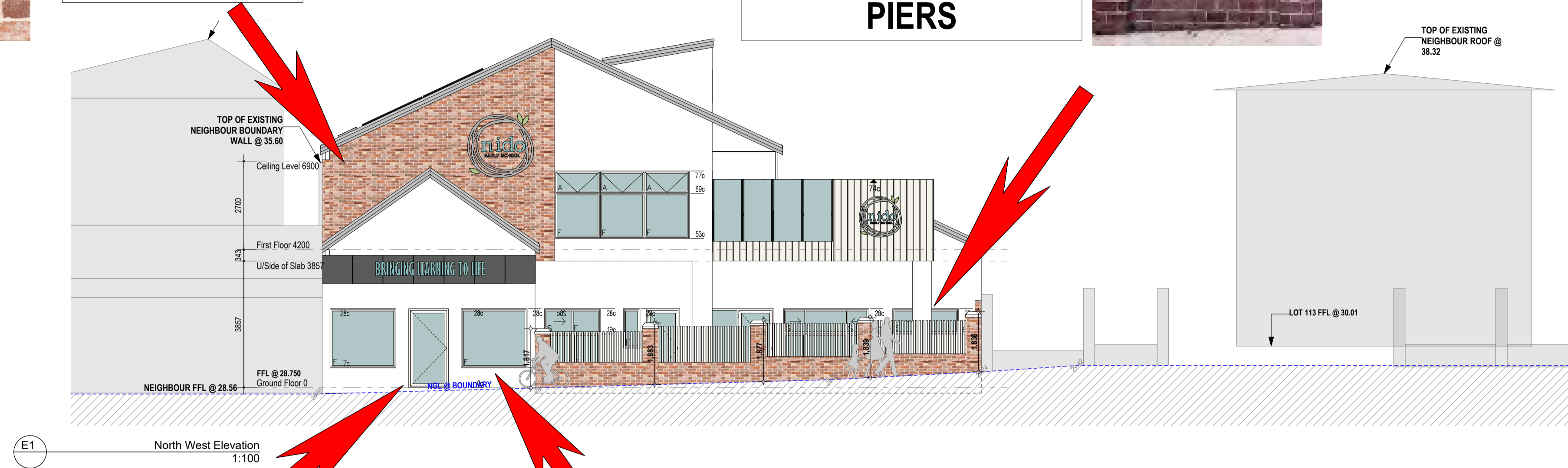
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**MIDLAND
BRICK
RECYCLED
BRICKS**

**BRICK FENCE W/
PAINTED ALUMIN
INFILL PANELS W/
CAPPING TO BRICK
PIERS**



**FRONT FACADE AS PER
"FIGURE 31 - SINGLE
STOREY SHOP"**

**ACRYLIC
RENDER DULUX
TERRACE WHITE**



**ZINCALUME
ROOF**



Revision	Description	Date
6.00	Development Approval	04.02.26
5.00	Heritage Comments	17.10.25
4.00	Heritage Comments	02.06.25
3.00	DRP	10.04.25
2.00	Changes	04.04.25
1.00	Concept	28.02.25

Project North:



Project Name
Childcare Centre

Project Address
Lot 112 (#752) Beaufort St, Mt Lawley

Drawing Title:
Elevations

Scale: 1:100 Sheet Size: A1

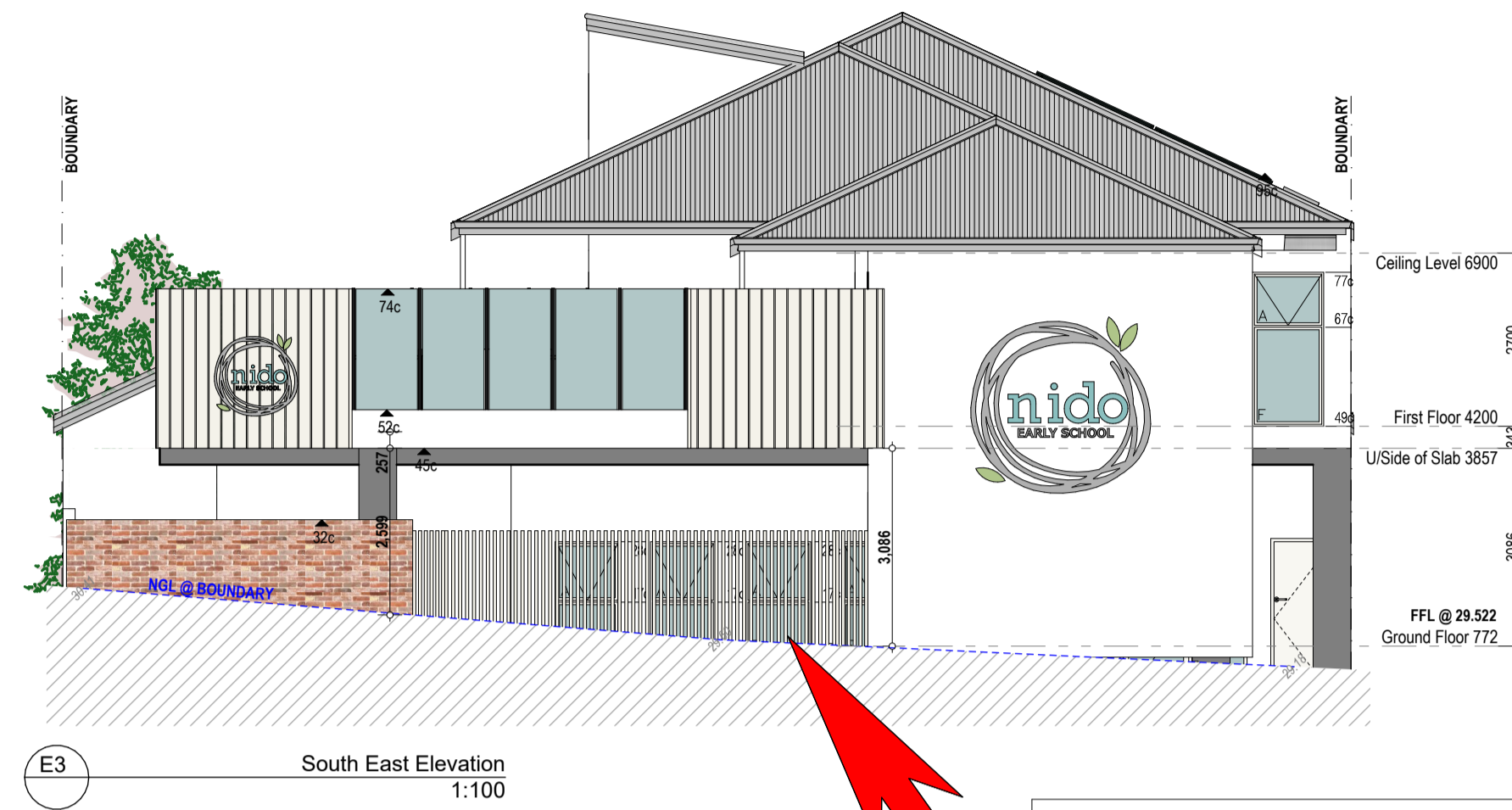
Project No: 22082 Revision Number: 6.00

Drawing No.: 10 of 14

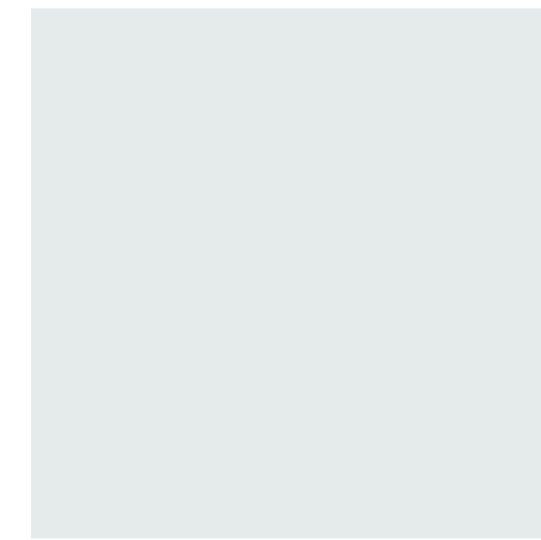
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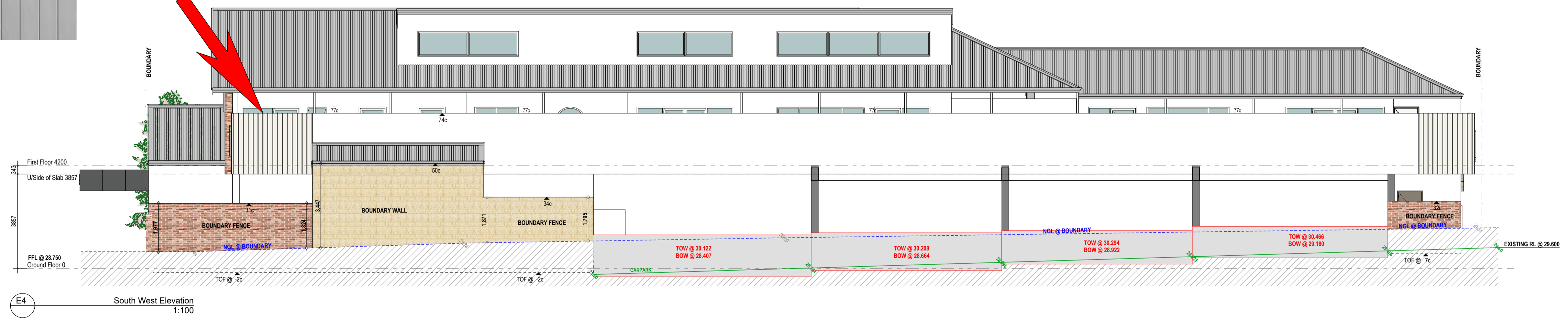
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**CAPRAL WINDOWS &
DOOR FRAMES WHITE
FLATT MAT (WIDE
PROFILE)**



**AXON CLADDING
PAINTED WHITE**



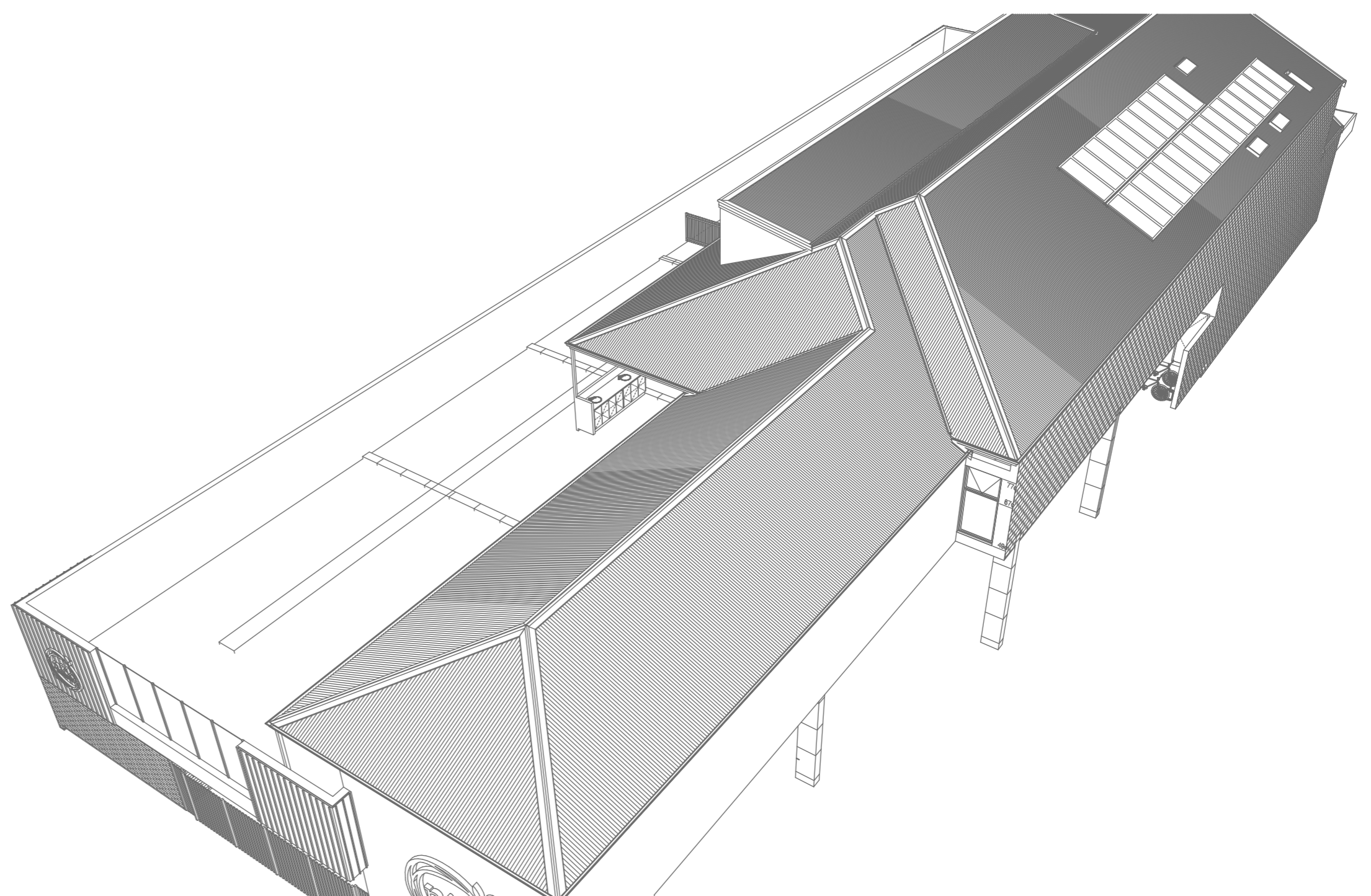
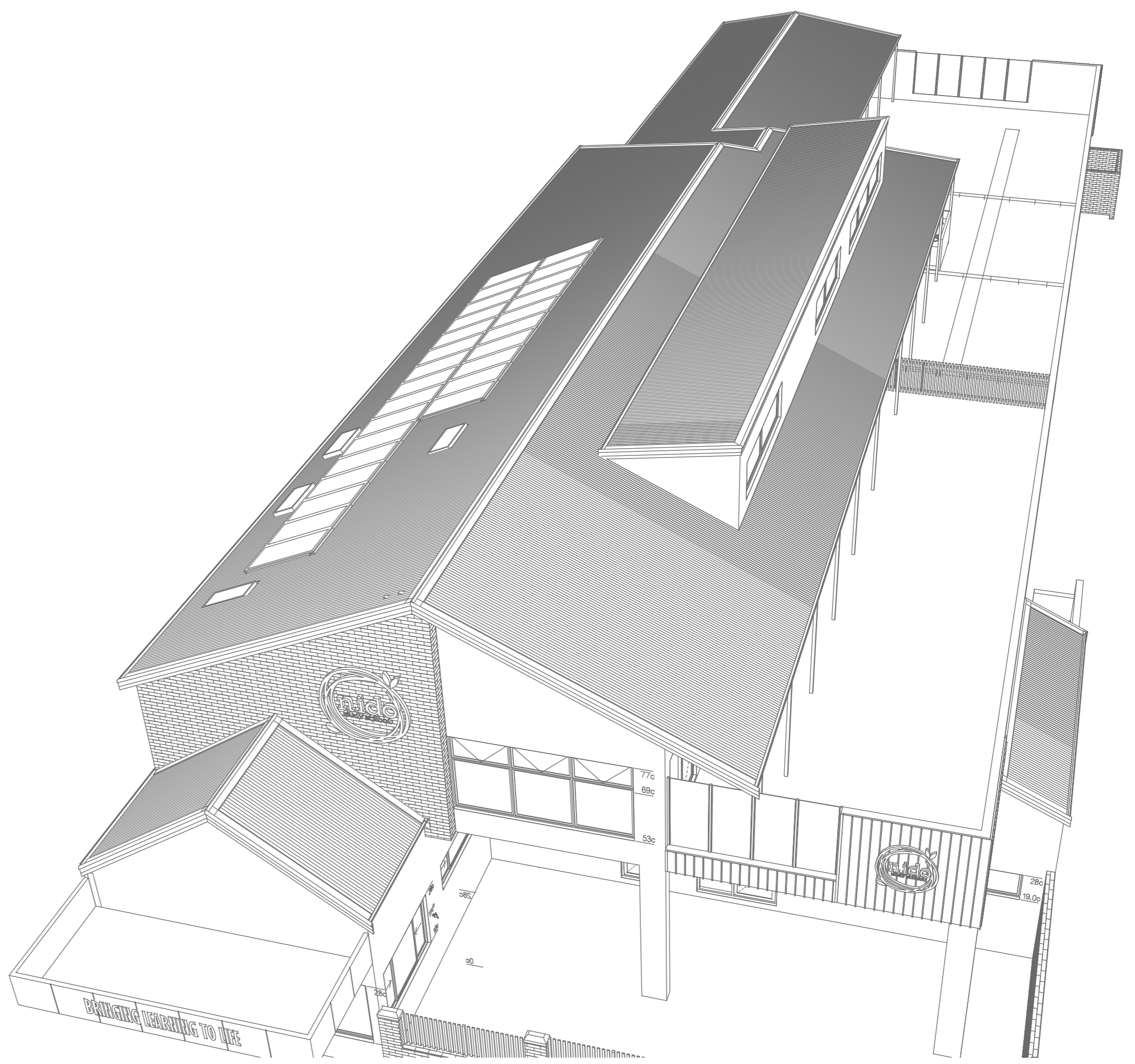
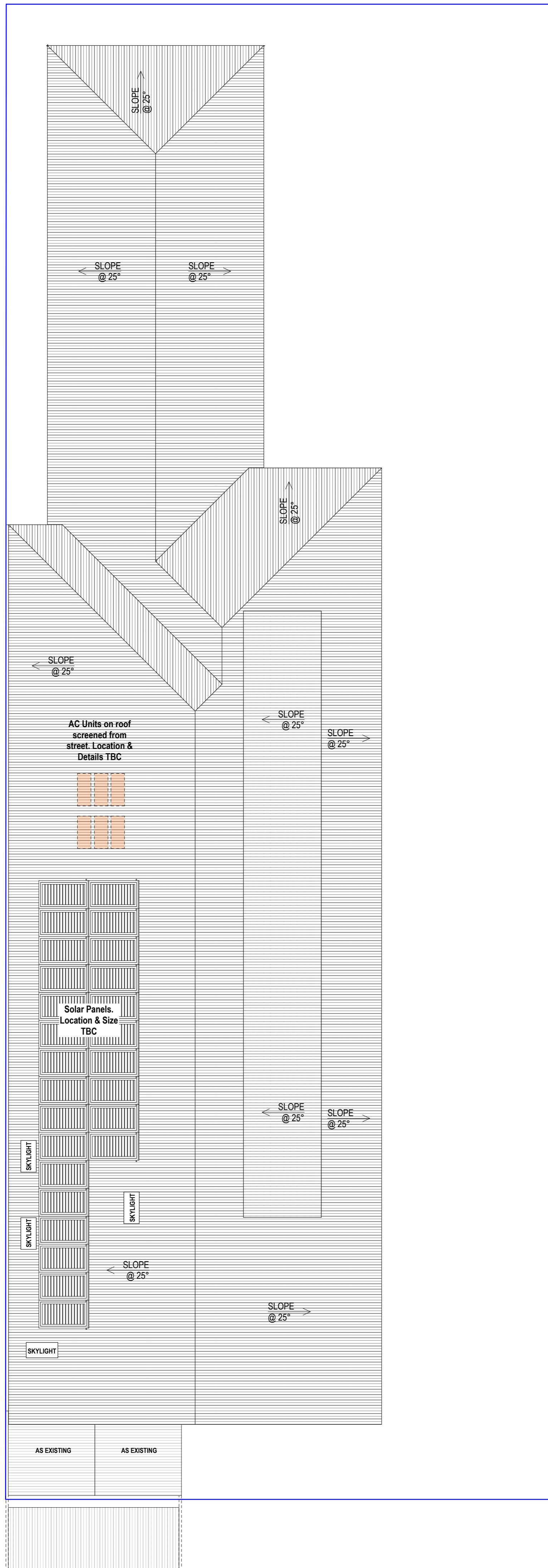
Revision	Description	Date
6.00	Development Approval	04.02.26
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4.00	Heritage Comments	02.06.25
3.00	DRP	10.04.25
2.00	Changes	04.04.25
1.00	Concept	28.02.25



Project Name
Childcare Centre
Project Address
Lot 112 (#752) Beaufort St, Mt Lawley
Drawing Title:
Elevations

Scale: 1:100	Sheet Size: A1
Project No: 22082	Revision Number: 6.00

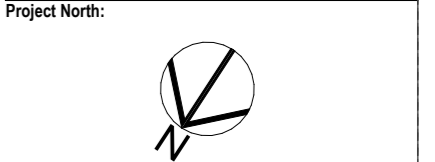
Drawing No.:
11 of 14



Our proposed childcare center has been thoughtfully designed with sustainability in mind. The building will include solar panels to support renewable energy use and reduce our carbon footprint. Roof structures will provide shading for windows, helping to naturally regulate indoor temperatures and reduce reliance on air conditioning. To further conserve energy, we are installing high-quality insulation and LED lighting throughout the facility. Water conservation measures will be implemented, alongside a dedicated recycling program to minimize waste. Additionally, bike bays will be provided to encourage environmentally friendly transport and promote a healthy lifestyle. Landscaping will feature native planting on the front verge, supporting local biodiversity and reducing water usage.

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1 Apr 2026
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Revision	Description	Date
6.00	Development Approval	04.02.26
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4.00	Heritage Comments	02.08.25
3.00	DRP	10.04.25
2.00	Changes	04.04.25
1.00	Concept	28.02.25



Project Name
Childcare Centre
Project Address
Lot 112 (#752) Beaufort St, Mt Lawley

Drawing Title
Roof Plan

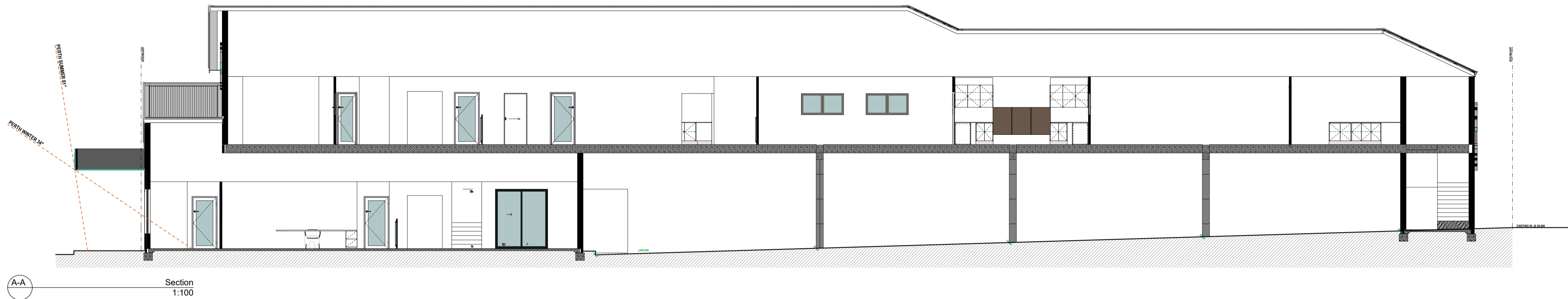
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Project No: 22082	Revision Number: 6.00

Drawing No.:
12 of 14

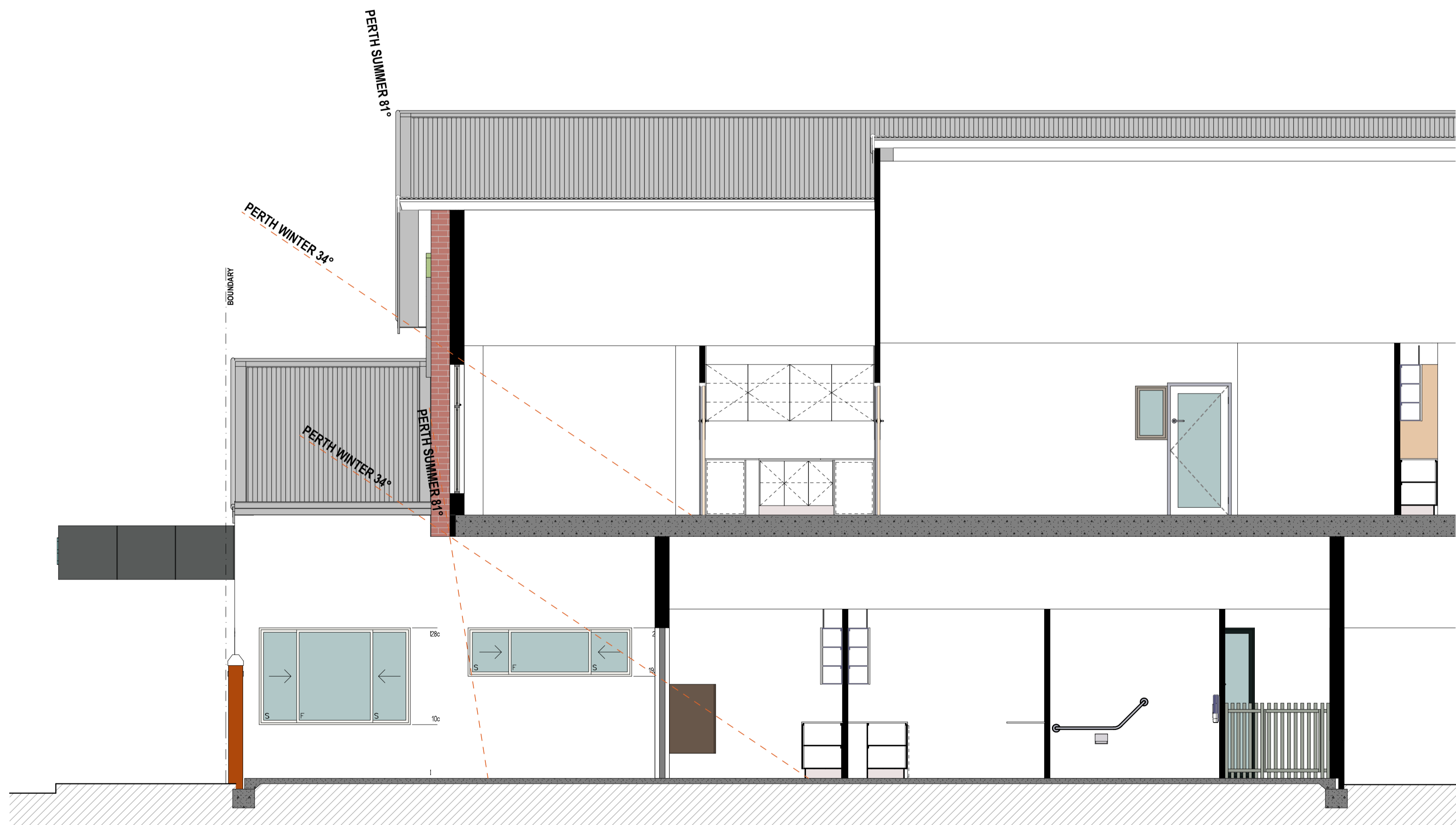


Unit: 3/1 Mulgill Road, Malaga WA 6000
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A-A Section
1:100



B-B Section
1:50

Our proposed childcare center has been thoughtfully designed with sustainability in mind. The building will include solar panels to support renewable energy use and reduce our carbon footprint. Roof structures will provide shading for windows, helping to naturally regulate indoor temperatures and reduce reliance on air conditioning. To further conserve energy, we are installing high-quality insulation and LED lighting throughout the facility. Water conservation measures will be implemented, alongside a dedicated recycling program to minimize waste. Additionally, bike bays will be provided to encourage environmentally friendly transport and promote a healthy lifestyle. Landscaping will feature native planting on the front verge, supporting local biodiversity and reducing water usage.

Revision	Description	Date
8.00	Development Approval	04.02.26
5.00	Heritage Comments	17.10.25
4.00	Heritage Comments	02.08.25
3.00	DRP	10.04.25
2.00	Changes	04.04.25
1.00	Concept	28.02.25

Project North:



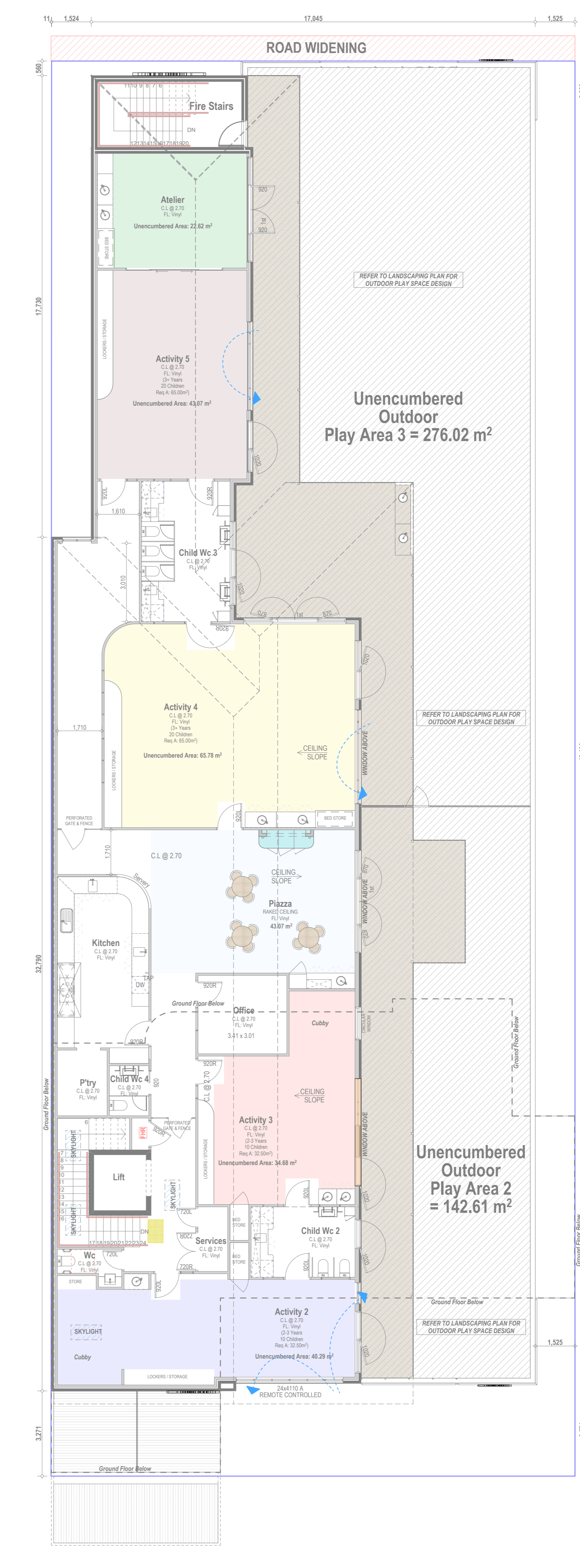
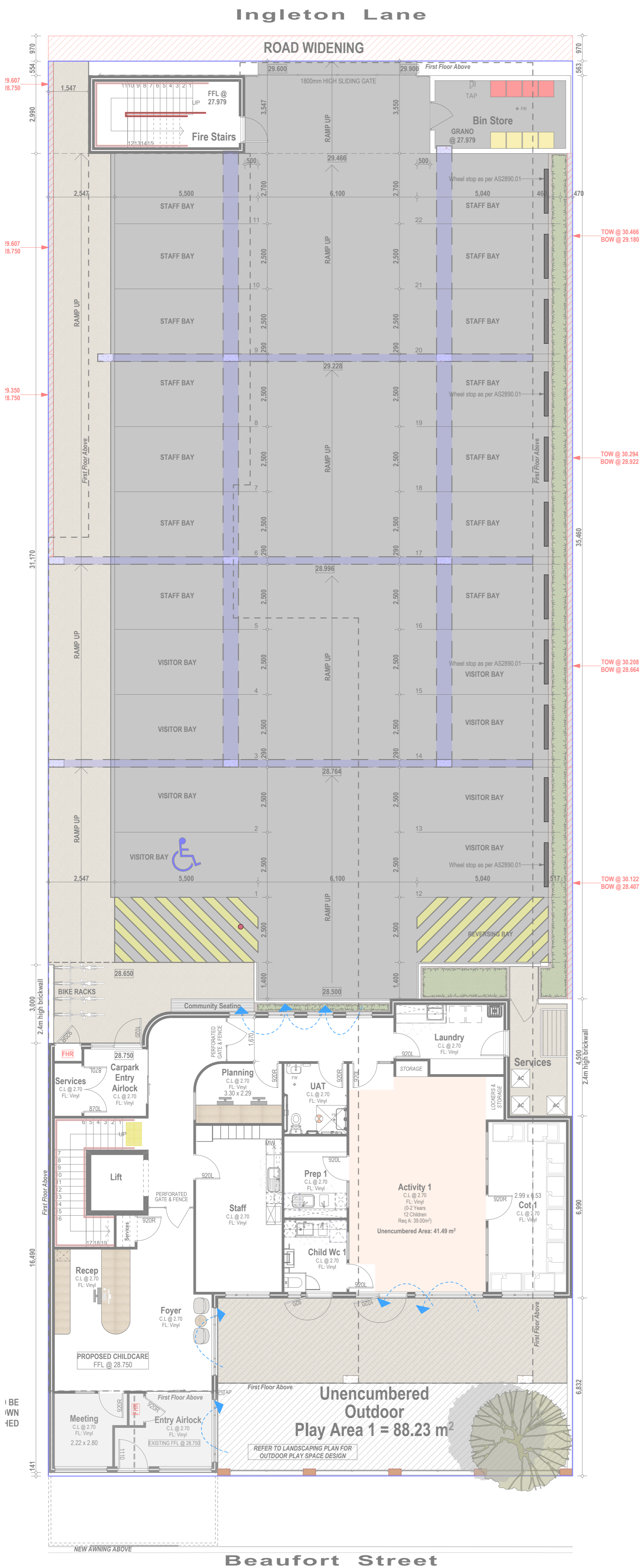
Project Name
Childcare Centre
Project Address
Lot 112 (#752) Beaufort St, Mt Lawley
Drawing Title:
Section & Solar Study

Scale:	Sheet Size:	A1
1:100, 1:50		
Project No: 22082	Revision Number: 6.00	

Drawing No.:
13 of 14



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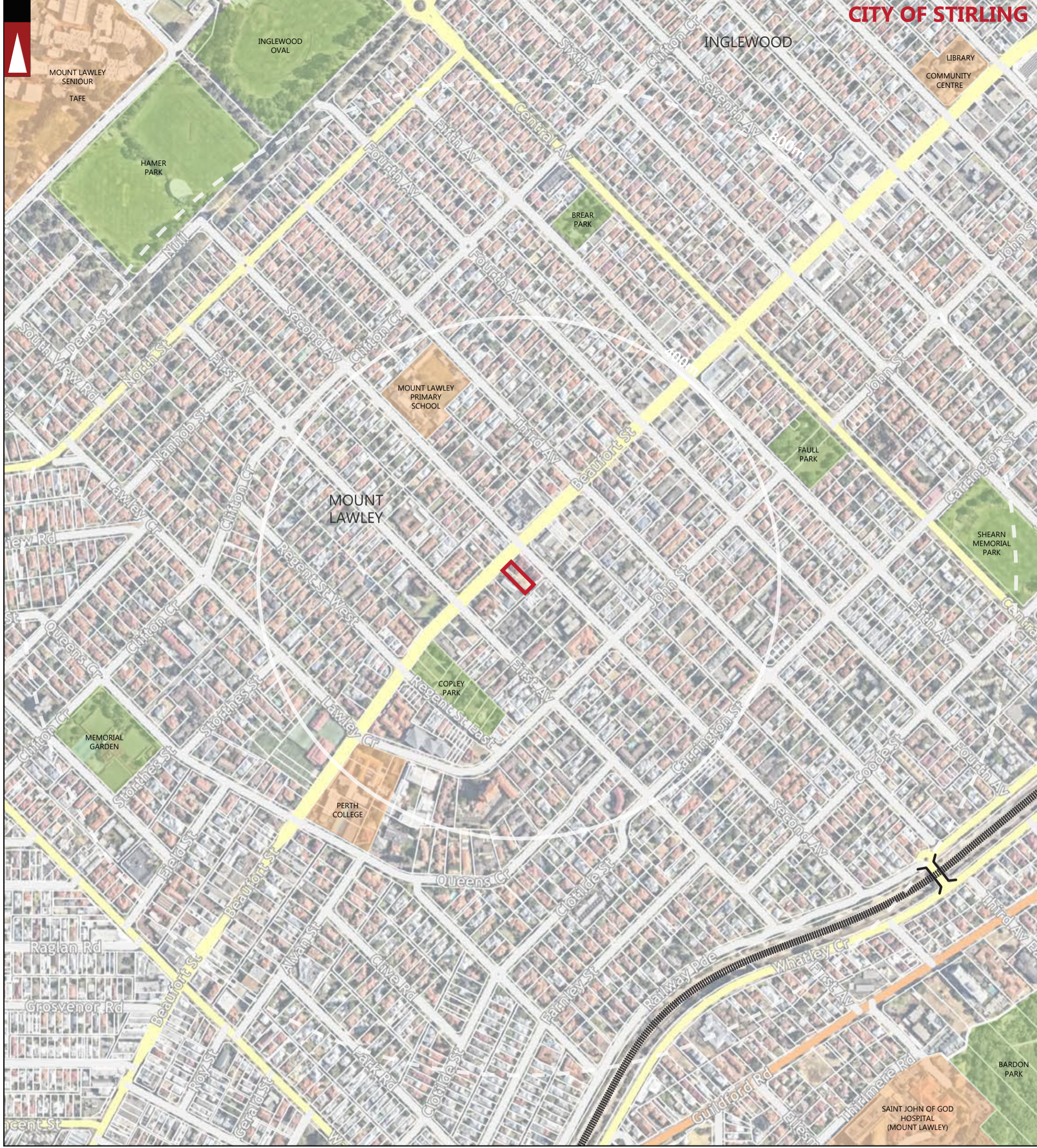



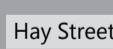







Revision	Description	Date
6.00	Development Approval	04.02.26
5.00	Heritage Comments	17.10.25
4.00	Heritage Comments	20.08.25
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2.00	Changes	04.04.25
1.00	Concept	26.02.25

Project Name	Date
Childcare Centre <td></td>	
Project Address <td></td>	
Lot 112 (#752) Beaufort St, Mt Lawley <td></td>	




Drawing Title:
Breeze Path Plan

Scale:	Sheet Size:
1:100	A1
Project No:	Revision Number:
22082	6.00
Drawing No.:	
14 of 14	




 PARKS AND RECREATION	 STREET NAME	 LOCATION BOUNDARY	LOCAL GOVERNMENT NAME
 WATERWAYS	 RAILWAY	 DISTANCE FROM LOCATION	CITY OF STIRLING
 PUBLIC PURPOSE	 ROAD BRIDGE	 NORTHBRIDGE	SUBURB NAME

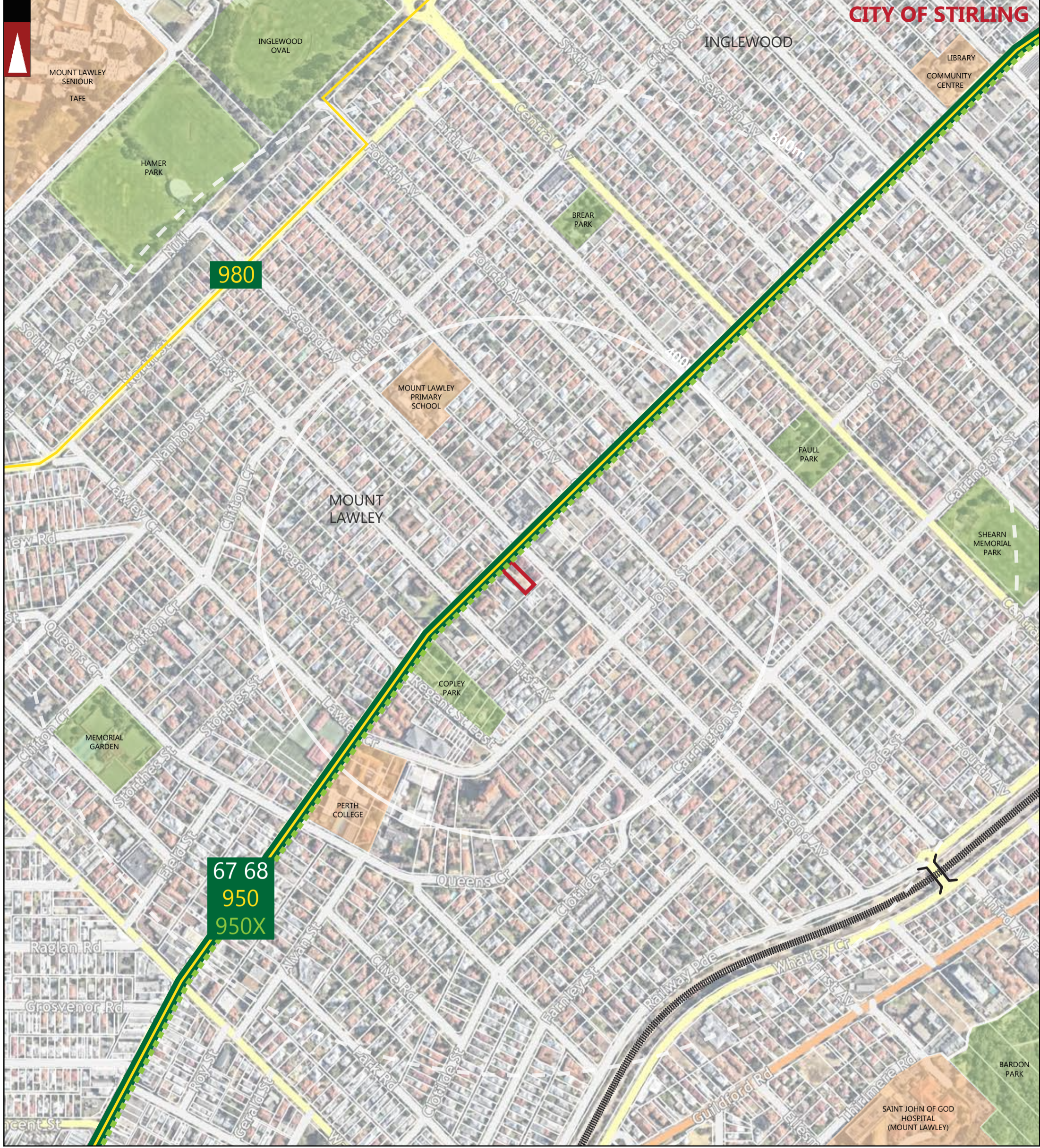
Roads

-  Freeway/Highway
-  Major Road
-  Minor Road

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LEGEND

			PROJECT: LOT 112 (#752) BEAUFORT STREET, MOUNT LAWLEY	DRAWN BY:	
			TITLE: LOCALITY PLAN - 800M RADIUS		
A	20-02-2026	ISSUED FOR REVIEW	DRAWING NUMBER: P004693_ S01	J.S.	
REV	DATE	AMENDMENT			



PARKS AND RECREATION	Hay Street STREET NAME	LOCATION BOUNDARY	BUS ROUTES	67 BUS ROUTE NUMBER
WATERWAYS	RAILWAY	DISTANCE FROM LOCATION	HIGH FREQUENCY BUS ROUTE	950 HIGH FREQUENCY BUS ROUTE
PUBLIC PURPOSE	ROAD BRIDGE	CITY OF STIRLING LOCAL GOVERNMENT NAME	LIMITED STOPS BUS ROUTE	950X LIMITED STOP ROUTE NUMBER
		NORTHBRIDGE SUBURB NAME		

LEGEND

Roads

- Freeway/Highway
- Major Road
- Minor Road

NOTE : FOR MORE INFORMATION REGARDING THE DESCRIPTION OF BUS ROUTES AND THEIR INDICATIVE PEAK AND OFF-PEAK FREQUENCIES REFER TO THE REPORT.

REV	DATE	AMENDMENT
A	20-02-2026	ISSUED FOR REVIEW

PROJECT:
LOT 112 (#752) BEAUFORT STREET, MOUNT LAWLEY

TITLE:
PUBLIC TRANSPORT PLAN - 800M RADIUS

DRAWING NUMBER:
P004693_ S03

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DRAWN BY:
 J.S.



	PARKS AND RECREATION		STREET NAME		LOCATION BOUNDARY		PRINCIPAL SHARED PATH (PSP)
	WATERWAYS		DISTANCE FROM LOCATION		CITY OF STIRLING		HIGH QUALITY SHARED PATH
	PUBLIC PURPOSE		RAILWAY		LOCAL GOVERNMENT NAME		OTHER SHARED PATH (SHARED BY PEDESTRIANS & CYCLISTS)
			ROAD BRIDGE		NORTHBRIDGE		PEDESTRIAN PATH WITHIN 400M RADIUS FROM THE SUBJECT SITE

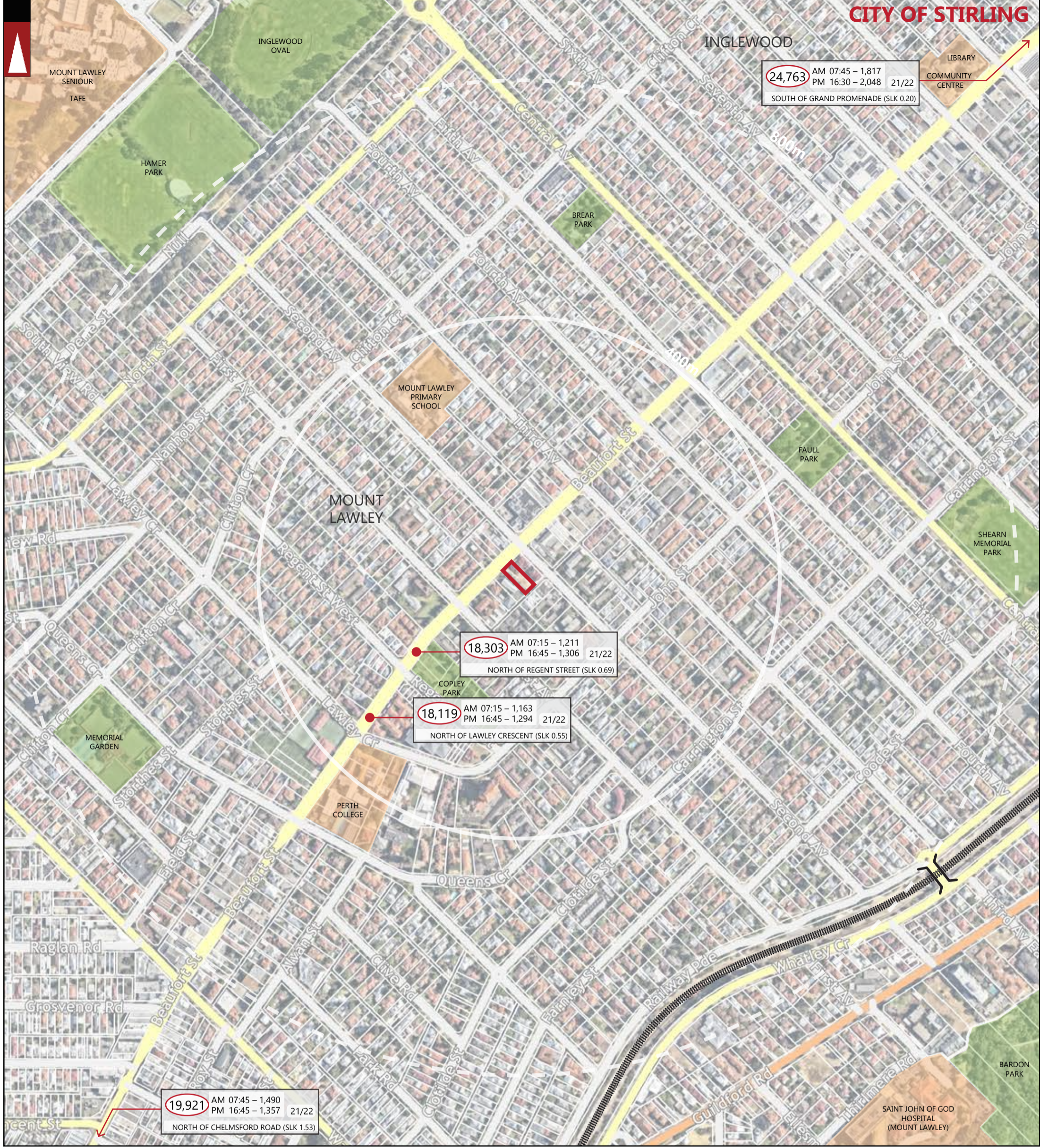
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Roads

- Freeway/Highway
- Major Road
- Minor Road

			PROJECT: LOT 112 (#752) BEAUFORT STREET, MOUNT LAWLEY	DRAWN BY:	 Premise <small>PART OF THE Amey GROUP</small>
			TITLE: PEDESTRIAN PATHS PLAN - 800M RADIUS		
			DRAWING NUMBER: P004693_ S04	J.S.	
REV	DATE	AMENDMENT			
A	20-02-2026	ISSUED FOR REVIEW			



PARKS AND RECREATION	Hay Street STREET NAME	LOCATION BOUNDARY	5,512 NUMBER OF VEHICLES PER DAY
WATERWAYS	RAILWAY	DISTANCE FROM LOCATION	AM 1145 – 381 PM 1630 – 480 NUMBER OF VEHICLES PER AM PEAK HOUR NUMBER OF VEHICLES PER PM PEAK HOUR
PUBLIC PURPOSE	ROAD BRIDGE	CITY OF STIRLING LOCAL GOVERNMENT NAME	2014 YEAR
		NORTHBRIDGE SUBURB NAME	EAST OF HARLOW ROAD LOCATION

Roads

- Freeway/Highway
- Major Road
- Minor Road

LEGEND

		PROJECT: LOT 112 (#752) BEAUFORT STREET, MOUNT LAWLEY	City of Stirling 1 Apr 2026 RECEIVED	DRAWN BY:	<p>Premise PART OF THE AMEY GROUP</p>
		TITLE: EXISTING TRAFFIC COUNTS - 800M RADIUS		J.S.	
A	20-02-2026	ISSUED FOR REVIEW			
REV	DATE	AMENDMENT	DRAWING NUMBER: P004693_ S05		



LOCATION BOUNDARY

Lewis Road ROAD NAME

1,389

Total Expected Traffic Generation from the proposed development

503

Total Expected Traffic Generation from Subject Site on the specific section of road - **IN and OUT** direction

Traffic Flow IN Direction

Traffic Flow OUT Direction

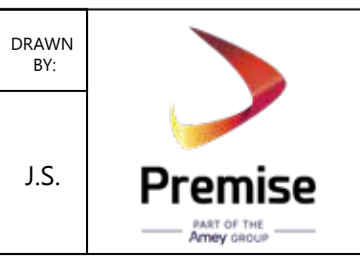
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NOTE: THE PLAN IS COURTESY OF GERMANO DESIGN

LEGEND

REV	DATE	AMENDMENT
A	20-02-2026	ISSUED FOR REVIEW

PROJECT: LOT 112 (#752) BEAUFORT STREET, MOUNT LAWLEY	DRAWN BY: J.S.
TITLE: TRAFFIC FLOW DIAGRAM - DAILY	
DRAWING NUMBER: P004693_ S06	





LOCATION BOUNDARY

Lewis Road ROAD NAME

Total Expected Traffic Generation from the proposed development

Total Expected Traffic Generation from Subject Site on the specific section of road - **IN and OUT** direction

Traffic Flow IN Direction

Traffic Flow OUT Direction

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NOTE: THE PLAN IS COURTESY OF GERMANO DESIGN

LEGEND

			PROJECT: LOT 112 (#752) BEAUFORT STREET, MOUNT LAWLEY	DRAWN BY:	 Premise <small>PART OF THE AMEY GROUP</small>
			TITLE: TRAFFIC FLOW DIAGRAM - AM PEAK		
			DRAWING NUMBER: P004693_ S07		
REV	DATE	AMENDMENT		J.S.	
A	20-02-2026	ISSUED FOR REVIEW			



LOCATION BOUNDARY

Lewis Road ROAD NAME

1,389

Total Expected Traffic Generation from the proposed development

503

Total Expected Traffic Generation from Subject Site on the specific section of road - **IN and OUT direction**

Traffic Flow IN Direction

Traffic Flow OUT Direction

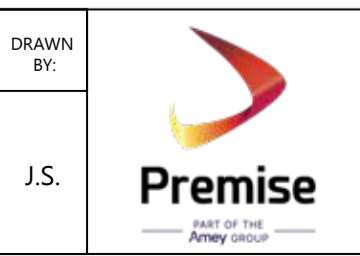
City of Stirling
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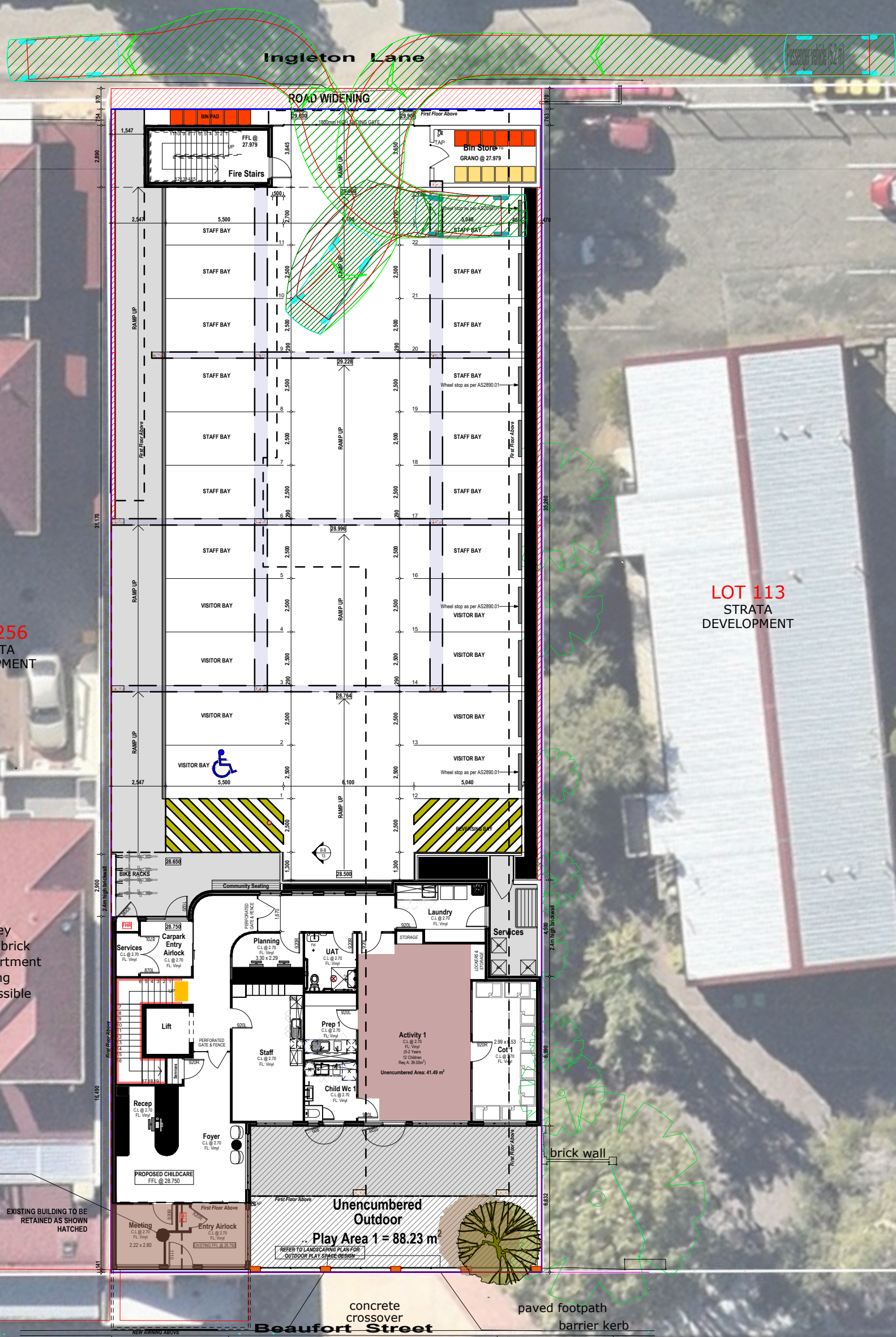
NOTE: THE PLAN IS COURTESY OF GERMANO DESIGN

LEGEND

REV	DATE	AMENDMENT
A	20-02-2026	ISSUED FOR REVIEW

PROJECT: LOT 112 (#752) BEAUFORT STREET, MOUNT LAWLEY	DRAWN BY: J.S.
TITLE: TRAFFIC FLOW DIAGRAM - PM PEAK	
DRAWING NUMBER: P004693_ S08	





LOT 256
STRATA
DEVELOPMENT

LOT 113
STRATA
DEVELOPMENT

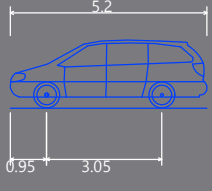
3 storey
rendered brick
& steel apartment
building
FL inaccessible

EXISTING BUILDING TO BE
RETAINED
FFL @ 28.750

EXISTING BUILDING TO BE
RETAINED AS SHOWN
HATCHED

concrete
crossover

paved footpath
barrier kerb




Passenger vehicle (5.2 m)
Overall Length 5.200m
Overall Width 1.940m
Overall Body Height 1.804m
Min Body Ground Clearance 0.295m
Track Width 1.840m
Lock to Lock Time 4.00s
Kerb to Kerb Turning Radius 6.300m

- Lot boundary
- Wheel Path (Forward Vehicle Motion)
- Vehicle Chassis Envelope (Forward Vehicle Motion)
- Wheel Path (Reverse Vehicle Motion)
- Vehicle Chassis Envelope (Reverse Vehicle Motion)

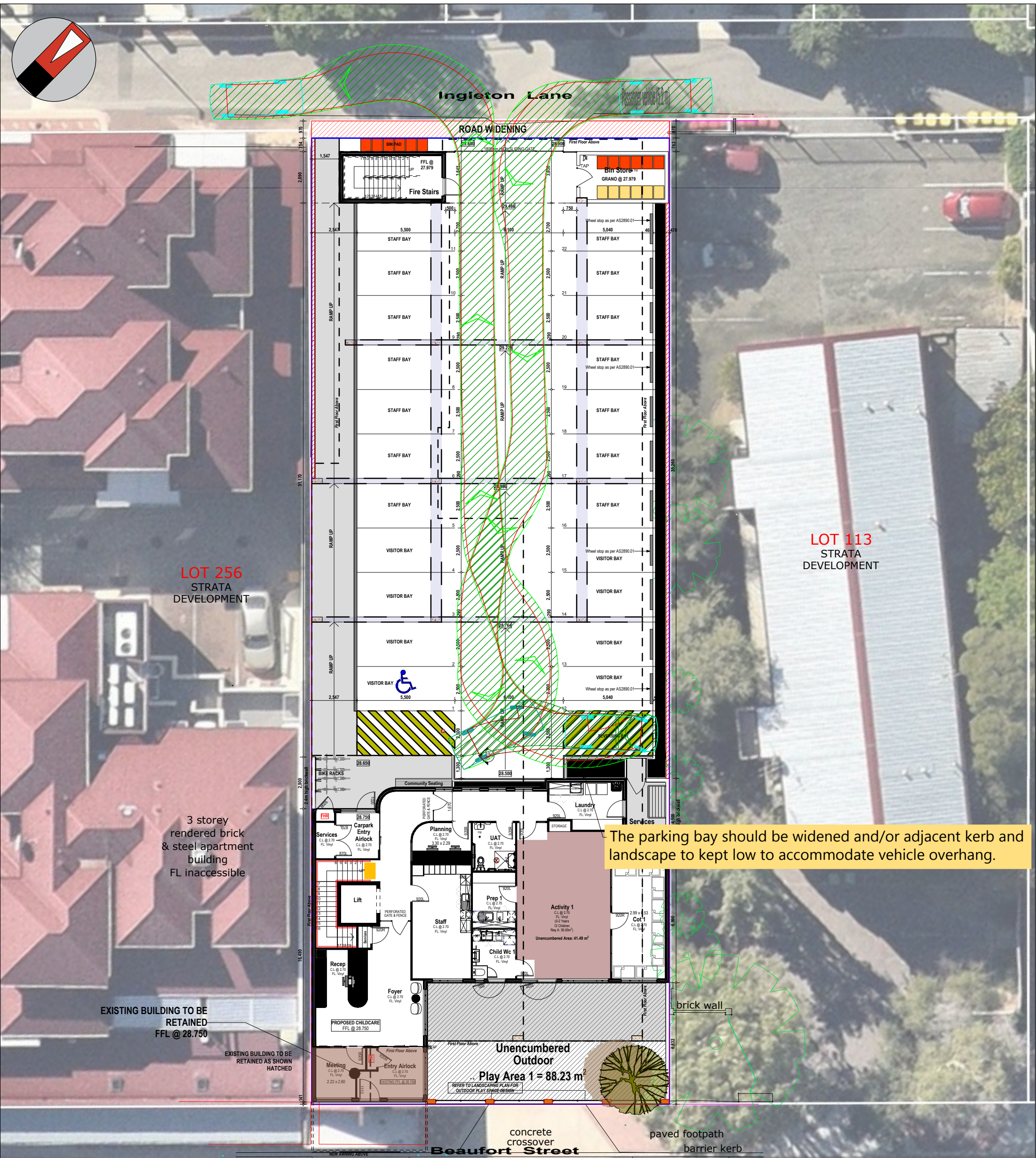
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1 Apr 2026
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			PROJECT: LOT 112 (#752) BEAUFORT STREET, MOUNT LAWLEY	DRAWN BY:	 Premise <small>PART OF THE AMEY GROUP</small>
			TITLE: Vehicle Turning Circle Plan - B99 Passenger Vehicle (5.2m)		
A	20-02-2026	ISSUED FOR REVIEW	DRAWING NUMBER: P004693_S20	J.S.	
NO	DATE	AMENDMENT			



Ingleton Lane



LOT 256
STRATA
DEVELOPMENT

LOT 113
STRATA
DEVELOPMENT

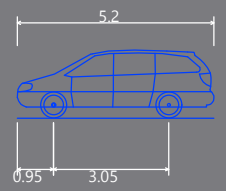
3 storey
rendered brick
& steel apartment
building
FL inaccessible

EXISTING BUILDING TO BE
RETAINED
FFL @ 28.750

EXISTING BUILDING TO BE
RETAINED AS SHOWN
HATCHED

The parking bay should be widened and/or adjacent kerb and landscape to kept low to accommodate vehicle overhang.

Beaufort Street




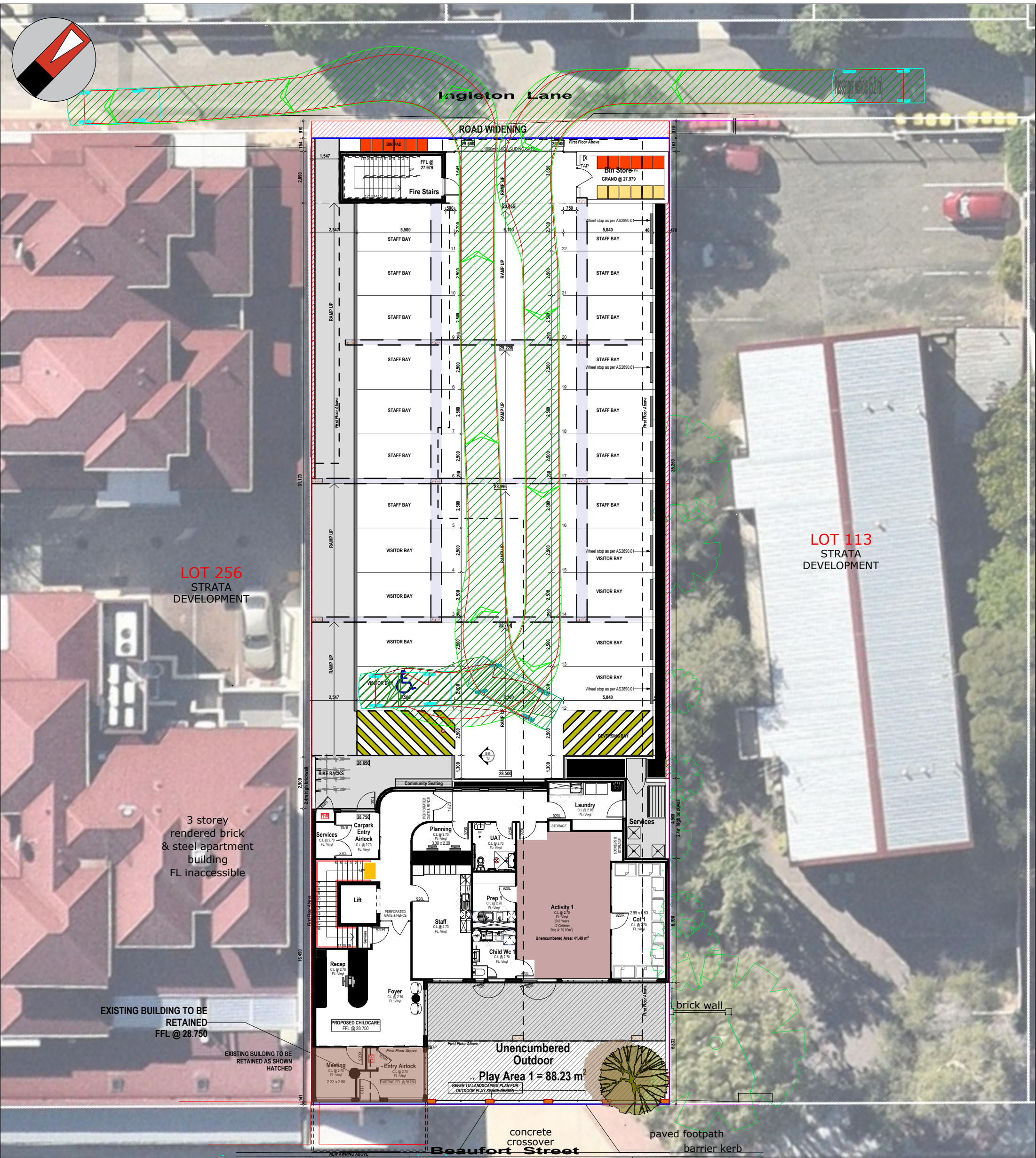
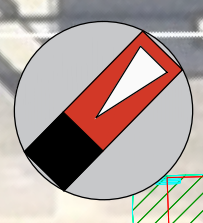
Passenger vehicle (5.2 m)
Overall Length 5.200m
Overall Width 1.940m
Overall Body Height 1.804m
Min Body Ground Clearance 0.295m
Track Width 1.840m
Lock to Lock Time 4.00s
Kerb to Kerb Turning Radius 6.300m

- Lot boundary
- Wheel Path (Forward Vehicle Motion)
- Vehicle Chassis Envelope (Forward Vehicle Motion)
- Wheel Path (Reverse Vehicle Motion)
- Vehicle Chassis Envelope (Reverse Vehicle Motion)

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			PROJECT: LOT 112 (#752) BEAUFORT STREET, MOUNT LAWLEY	DRAWN BY:	
			TITLE: Vehicle Turning Circle Plan - B99 Passenger Vehicle (5.2m)		
A	20-02-2026	ISSUED FOR REVIEW	DRAWING NUMBER: P004693_S21	J.S.	
NO	DATE	AMENDMENT			



LOT 256
STRATA
DEVELOPMENT

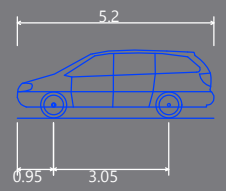
LOT 113
STRATA
DEVELOPMENT

3 storey
rendered brick
& steel apartment
building
FL inaccessible

EXISTING BUILDING TO BE
RETAINED
FFL @ 28.750

EXISTING BUILDING TO BE
RETAINED AS SHOWN
HATCHED

Beaufort Street



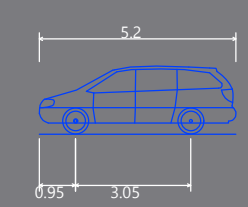
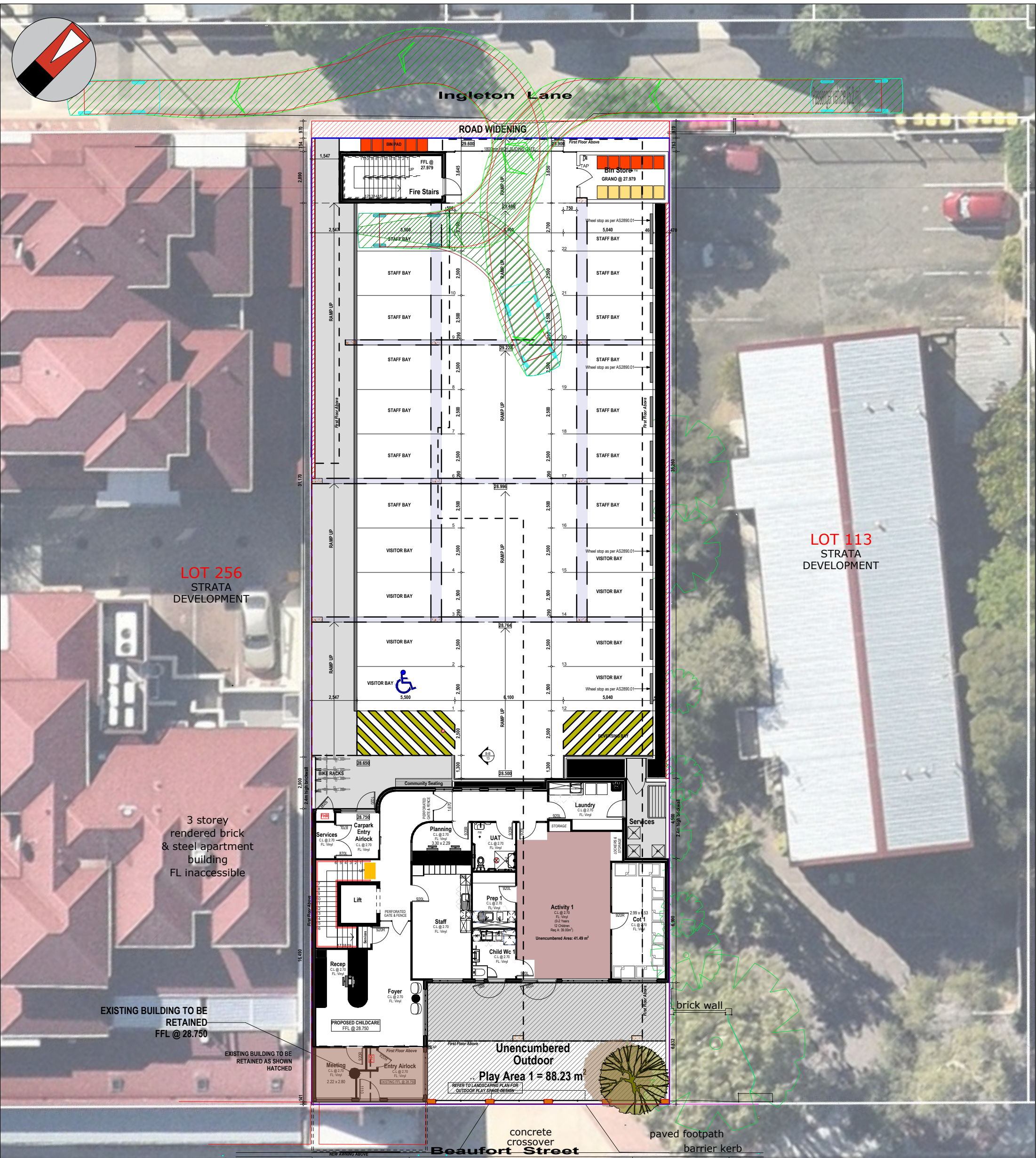
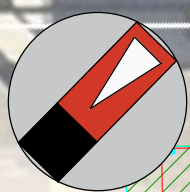
- Passenger vehicle (5.2 m)
- Overall Length 5.200m
- Overall Width 1.940m
- Overall Body Height 1.804m
- Min Body Ground Clearance 0.295m
- Track Width 1.840m
- Lock to Lock Time 4.00s
- Kerb to Kerb Turning Radius 6.300m

- Lot boundary
- Wheel Path (Forward Vehicle Motion)
- Vehicle Chassis Envelope (Forward Vehicle Motion)
- Wheel Path (Reverse Vehicle Motion)
- Vehicle Chassis Envelope (Reverse Vehicle Motion)

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			PROJECT: LOT 112 (#752) BEAUFORT STREET, MOUNT LAWLEY TITLE: Vehicle Turning Circle Plan - B99 Passenger Vehicle (5.2m) DRAWING NUMBER: P004693_S22	DRAWN BY: J.S.	 Premise <small>PART OF THE AMEY GROUP</small>
A	20-02-2026	ISSUED FOR REVIEW			
NO	DATE	AMENDMENT			



Passenger vehicle (5.2 m)
 Overall Length 5.200m
 Overall Width 1.940m
 Overall Body Height 1.804m
 Min Body Ground Clearance 0.295m
 Track Width 1.840m
 Lock to Lock Time 4.00s
 Kerb to Kerb Turning Radius 6.300m

- Lot boundary
- Wheel Path (Forward Vehicle Motion)
- Vehicle Chassis Envelope (Forward Vehicle Motion)
- Wheel Path (Reverse Vehicle Motion)
- Vehicle Chassis Envelope (Reverse Vehicle Motion)

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			PROJECT: LOT 112 (#752) BEAUFORT STREET, MOUNT LAWLEY	DRAWN BY:	
			TITLE: Vehicle Turning Circle Plan - B99 Passenger Vehicle (5.2m)		
draft	18-02-2026	ISSUED FOR REVIEW	DRAWING NUMBER: P004693_S23	J.S.	
NO	DATE	AMENDMENT			

**PROPOSED CHILDCARE CENTRE
752 BEAUFORT STREET
MT LAWLEY**

ENVIRONMENTAL ACOUSTIC ASSESSMENT

MARCH 2026

OUR REFERENCE: 36155-1-26063

DOCUMENT CONTROL PAGE

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ENVIRONMENTAL ACOUSTIC ASSESSMENT
PROPOSED CHILD CARE CENTRE
MT LAWLEY

Job No: 26063

Document Reference: 36155-1-26063

FOR

GERMANO DESIGNS


DOCUMENT INFORMATION

Author:	Geoff Harris	Checked By:	Tim Reynolds
Date of Issue:	17 March 2026		

REVISION HISTORY

Revision	Description	Date	Author	Checked

DOCUMENT DISTRIBUTION

Version No.	Destination
1	Germano Designs 

This report has been prepared in accordance with the scope of services and on the basis of information and documents provided to Herring Storer Acoustics by the client. To the extent that this report relies on data and measurements taken at or under the times and conditions specified within the report and any findings, conclusions or recommendations only apply to those circumstances and no greater reliance should be assumed. The client acknowledges and agrees that the reports or presentations are provided by Herring Storer Acoustics to assist the client to conduct its own independent assessment.

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1.	INTRODUCTION	1
2.	SUMMARY	1
3.	CRITERIA	2
4.	PROPOSAL	4
5.	MODELLING	4
6.	ASSESSMENT	5
7.	CONCLUSION	7

APPENDICIES

A	PLANS
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1. INTRODUCTION

Herring Storer Acoustics were commissioned by Germano Designs to undertake an acoustic assessment of noise emissions associated with the proposed child care to be located at 752 Beaufort Street, Mt Lawley.

The report considers noise received at the neighbouring premises from the proposed development for compliance with the requirements of the *Environmental Protection (Noise) Regulations 1997*. This report considers noise emissions from:

- Children playing within the outside play areas of the centre.
- Mechanical Plant.

We note that from information received from DWER, the bitumised area would be considered as a road, thus noise relating to motor vehicles is exempt from the *Environmental Protection (Noise) Regulations 1997*. We note that these noise sources are rarely critical in the determination of compliance. However, as requested by council and for completeness, they have been included in the assessment, for information purposes only.

For information, a plan of the proposed development is attached in Appendix A.

2. SUMMARY

Noise received at the neighbouring residences from the outdoor play areas would comply with the requirements of the *Environmental Protection (Noise) Regulations 1997*, provided outdoor play is limited to the day period (i.e. after 7am).

Noise from the mechanical services has also been assessed to comply with the relevant criteria given the implementation of a barrier. However, as the design of the mechanical services has not been undertaken at this stage of the project, it is recommended that the mechanical services design be reviewed for compliance with the Regulatory requirements.

It is noted that noise associated with cars movements and cars starting are exempt from complying with the Regulations. However, noise emissions from car doors are not strictly exempt from the Regulations. Noise received at the existing neighbouring residences from these noise sources would comply with the Regulatory requirements, at all times given the conditions listed below.

Thus, noise emissions from the proposed development, would be deemed to comply with the requirements of the *Environmental Protection (Noise) Regulations 1997* for the proposed hours of operation, with the inclusion of the following:

- 1 Although the proposed facility would open before 7 am (i.e. during the night period), the outdoor play area would not be used until after 7am. Thus, noise received at the neighbouring existing residences from the outdoor play area needs to comply with the assigned day period noise level.
- 2 For the outdoor play area, only proposed boundary fences would be required for the ground floor play area and a 1200 mm barrier from the first floor play area as shown on the plans in Appendix A would be required.
- 3 No car bays would need to be restricted.

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3. CRITERIA

The allowable noise level at the surrounding locales is prescribed by the *Environmental Protection (Noise) Regulations 1997*. Regulations 7 & 8 stipulate maximum allowable external noise levels. For highly sensitive area of a noise sensitive premises this is determined by the calculation of an influencing factor, which is then added to the base levels shown below in Table 3.1. The influencing factor is calculated for the usage of land within two circles, having radii of 100m and 450m from the premises of concern. For other areas within a noise sensitive premises, the assigned noise levels are fixed throughout the day, as listed in Table 3.1.

TABLE 3.1 - BASELINE ASSIGNED OUTDOOR NOISE LEVEL

Premises Receiving Noise	Time of Day	Assigned Level (dB)		
		L _{A10}	L _{A1}	L _{Amax}
Noise sensitive premises: highly sensitive area	0700 - 1900 hours Monday to Saturday (Day)	45 + IF	55 + IF	65 + IF
	0900 - 1900 hours Sunday and Public Holidays (Sunday / Public Holiday Day)	40 + IF	50 + IF	65 + IF
	1900 - 2200 hours all days (Evening)	40 + IF	50 + IF	55 + IF
	2200 hours on any day to 0700 hours Monday to Saturday and 0900 hours Sunday and Public Holidays (Night)	35 + IF	45 + IF	55 + IF
Commercial Premises	All Hours	60	75	80
Noise sensitive premises: any area other than highly sensitive area	All hours	60	75	80

Note: L_{A10} is the noise level exceeded for 10% of the time.
 L_{A1} is the noise level exceeded for 1% of the time.
 L_{Amax} is the maximum noise level.
 IF is the influencing factor.

Under the Regulations, a highly sensitive area means that area (if any) of noise sensitive premises comprising –

- (a) A building, or a part of a building, on the premises that is used for a noise sensitive purpose; and
- (b) Any other part of the premises within 15 m of that building or that part of the building.

It is a requirement that received noise be free of annoying characteristics (tonality, modulation and impulsiveness), defined below as per Regulation 9.

“impulsiveness” means a variation in the emission of a noise where the difference between L_{Apeak} and L_{Amax(Slow)} is more than 15 dB when determined for a single representative event;

“modulation” means a variation in the emission of noise that –

- (a) is more than 3 dB L_{Afast} or is more than 3 dB L_{Afast} in any one-third octave band;
- (b) is present for more at least 10% of the representative assessment period; and
- (c) is regular, cyclic and audible;



“tonality”

means the presence in the noise emission of tonal characteristics where the difference between –

- (a) the A-weighted sound pressure level in any one-third octave band; and
- (b) the arithmetic average of the A-weighted sound pressure levels in the 2 adjacent one-third octave bands,

is greater than 3 dB when the sound pressure levels are determined as $L_{Aeq,T}$ levels where the time period T is greater than 10% of the representative assessment period, or greater than 8 dB at any time when the sound pressure levels are determined as L_{ASlow} levels.

Where the noise emission is not music, if the above characteristics exist and cannot be practicably removed, then any measured level is adjusted according to Table 3.2 below.

TABLE 3.2 - ADJUSTMENTS TO MEASURED LEVELS

Where tonality is present	Where modulation is present	Where impulsiveness is present
+5 dB(A)	+5 dB(A)	+10 dB(A)

Note: These adjustments are cumulative to a maximum of 15 dB.

The neighbouring locations have been identified as

- R1 – Residents to the West.
- R2 – Residents to the South
- R3 – Residents to the East
- R4 – Residents to the North

Locations R1 to R3 would have an influencing factor of +7 dB due to Beaufort Street being within 100m and having 20% commercial in the outer circle.

TABLE 3.3 - ASSIGNED OUTDOOR NOISE LEVEL

Premises Receiving Noise	Time of Day	Assigned Level (dB)		
		L_{A10}	L_{A1}	L_{Amax}
R1-R4	0700 - 1900 hours Monday to Saturday (Day)	52	62	72
	0900 - 1900 hours Sunday and Public Holidays (Sunday / Public Holiday Day)	47	57	72
	1900 - 2200 hours all days (Evening)	47	57	62
	2200 hours on any day to 0700 hours Monday to Saturday and 0900 hours Sunday and Public Holidays (Night)	42	52	62

Note: L_{A10} is the noise level exceeded for 10% of the time.
 L_{A1} is the noise level exceeded for 1% of the time.
 L_{Amax} is the maximum noise level.



4. PROPOSAL

From information supplied, we understand that the child care centre normal hours of operations would likely be between 0630 and 1800 hours, Monday to Friday (closed on public holidays). It is understood that the proposed childcare centre will cater for 72 children.

It is noted that although the proposed child care centre would open before 7 am (i.e. during the night period), the outdoor play area would not be intended to be used until after 7am.

Similarly, mechanical plant would possibly operate all hours of the day.

5. MODELLING

To assess the noise received at the neighbouring premises from the proposed development, noise modelling was undertaken using the noise modelling program SoundPlan.

Calculations were carried out using the DWER's weather conditions, which relate to worst case noise propagation, as stated in the Department of Environment Regulation "*Draft Guidance on Environmental Noise for Prescribed Premises*". These conditions include winds blowing from sources to the receiver(s).

Calculations were based on the sound power levels used in the calculations are listed in Table 5.1, as well as plans provided by the client.

TABLE 5.1 – SOUND POWER LEVELS

Item	Sound Power Level, dB(A)
Children Playing	<24 months 78 (per 10 children) Between 24 and 36 months 85 (per 10 children) >36 months 87 (per 10 children)
Car Moving in Car Park	79
Car Starting	85
Door Closing	87
Kitchen/Bathroom Exhaust (2x)	71 each
Childcare Air conditioning condensing Unit (3x)	73 each, "max" operation 65 each, "quiet" operation

Notes:

- 1 Acoustic modelling of outdoor play noise was made, based on 72 children within the outdoor play area (i.e. worst case scenario) broken down as follows:
 - 0-2 years – 12 places
 - 2-3 years – 20 places
 - 3+ years – 40 places
- 2 The noise level for the mechanical plant has been based on the sound power levels used for previous assessment of child care centres. From other studies, we understand that the noise associated with the condensing units would be conservative.
- 3 For this child care centre, the mechanical plant units have been placed within the proposed AC area.
- 4 For the outdoor play area, a 1200mm high barrier for the first floor as shown on the drawings has been implemented.
- 5 To determine the restriction to the parking, a point noise source was located in each car bay.



- 6 Modelling shows that noise received at the neighbouring residences from car doors closing would comply with the assigned noise level for the day period.
- 7 To achieve compliance with the night period, no car bays would need to be restricted.
- 8 Calculations were undertaken for the receivers at 1.5 metres above the ground level.
- 9 Noise modelling was undertaken to a number of different receiver locations for each of the neighbouring residences, noting residents to the east are two storey. However, to simplify the assessment, only the noise level in the worst case location (i.e. highest noise level), have been listed.

6. ASSESSMENT

The tables below show the assessment of noise emissions of concern from the operation. Standard building construction will be sufficient to ensure that noise from inside the building will meet the regulations.

The resultant noise levels at the neighbouring residence from children playing outdoors and mechanical plant are tabulated in Table 6.1.

From previous measurements, noise emissions from children playing does not contain any annoying characteristics, however mechanical plant emissions would be considered tonal and attract a +5 dB(A) Penalty. Noise emissions from outdoor play needs to comply with the assigned L_{A10} noise levels.

**TABLE 6.1 - ACOUSTIC MODELLING RESULTS FOR L_{A10} CRITERIA
 OUTDOOR PLAY AREAS AND MECHANICAL PLANT**

Neighbouring Premises	Calculated Noise Level (dB(A))	
	Children Playing	Mechanical Plant
R1 Residential	41	35 (40)
R2 Residential	40	19 (24)
R3 Residential	50	33 (38)
R4 Residential	41	24 (29)

() Includes +5 dB(A) penalty for tonality

With regards to noise associated with cars within the parking area, resultant noise levels are tabulated in Tables 6.2 and 6.3. It is noted that noise emissions from a moving car being an L_{A1} noise level, with noise emissions from cars starting and doors closing being an L_{Amax} noise level.

Based on the definitions of tonality, noise emissions from car movements and car starts, being an L_{A1} and L_{Amax} respectively, being present for less than 10% of the time, would not be considered tonal. Thus, no penalties would be applicable, and the assessment would be as listed in Table 6.2 (Car Moving) and Table 6.3 (Car Starting). However, noise emissions from car doors closing could be impulsive, hence the +10dB penalty has been included in the assessment.

**TABLE 6.2 - ACOUSTIC MODELLING RESULTS L_{A1} CRITERIA
 CAR MOVING**

Neighbouring Premises	Calculated Noise Level (dB(A))
R1 Residential	35
R2 Residential	19
R3 Residential	33
R4 Residential	24

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**TABLE 6.3 - ACOUSTIC MODELLING RESULTS L_{Amax} CRITERIA
CAR STARTING / DOOR CLOSING**

Neighbouring Premises	Calculated Noise Level (dB(A))			
	Car Start		Car Door	
	Day Period	Night Period	Day Period	Night Period
R1 Residential	43	N/A	46 [56]	46 [56]
R2 Residential	28	N/A	31 [41]	31 [41]
R3 Residential	46	N/A	49 [59]	49 [59]
R4 Residential	46	N/A	49 [59]	49 [59]

[] Includes +10 dB(A) penalty for impulsiveness.

Tables 6.4 to 6.10 summarise the applicable Assigned Noise Levels, and assessable noise level emissions for each identified noise.

**TABLE 6.4 – ASSESSMENT OF L_{A10} NOISE LEVEL EMISSIONS
OUTDOOR PLAY (DAY PERIOD)**

Location	Assessable Noise Level dB(A)	Applicable Assigned Noise Level (dB(A))	Exceedance to Assigned Noise Level
R1 Residential	41	52	Complies
R2 Residential	40	52	Complies
R3 Residential	50	52	Complies
R4 Residential	41	52	Complies

**TABLE 6.5 – ASSESSMENT OF L_{A10} DAY PERIOD NOISE LEVEL EMISSIONS
MECHANICAL PLANT**

Location	Assessable Noise Level dB(A)	Applicable Assigned Noise Level (dB(A))	Exceedance to Assigned Noise Level
R1 Residential	40	42	Complies
R2 Residential	24	42	Complies
R3 Residential	38	42	Complies
R4 Residential	29	42	Complies

**TABLE 6.6 – ASSESSMENT OF L_{A1} NIGHT PERIOD NOISE LEVEL EMISSIONS
CAR MOVEMENTS**

Location	Assessable Noise Level dB(A)	Applicable Assigned Noise Level (dB(A))	Exceedance to Assigned Noise Level
R1 Residential	35	52	Complies
R2 Residential	19	52	Complies
R3 Residential	33	52	Complies
R4 Residential	24	52	Complies

**TABLE 6.7 – ASSESSMENT OF L_{Amax} Day PERIOD NOISE LEVEL EMISSIONS
CAR START**

Location	Assessable Noise Level dB(A)	Applicable Assigned Noise Level (dB(A))	Exceedance to Assigned Noise Level
R1 Residential	43	72	Complies
R2 Residential	28	72	Complies
R3 Residential	46	72	Complies
R4 Residential	46	72	Complies

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TABLE 6.8 – ASSESSMENT OF L_{Amax} NIGHT PERIOD NOISE LEVEL EMISSIONS

CAR DOOR			
Location	Assessable Noise Level dB(A)	Applicable Assigned Noise Level (dB(A))	Exceedance to Assigned Noise Level
R1 Residential	56	62	Complies
R2 Residential	41	62	Complies
R3 Residential	59	62	Complies
R4 Residential	59	62	Complies

7. CONCLUSION

Noise received at the neighbouring residences from the outdoor play area would comply during the day period, thus, outdoor play would be limited to the day period (i.e. – after 7am).

Noise received at the neighbouring residences from the mechanical plant would comply at all hours. Note, once mechanical selection and placement is finalised, it is recommended than an updated acoustic report be provided to ensure the selection complies.

Noise received at the neighbouring residences from noise associated with vehicles would also comply at all hours.

Thus, noise emissions from the proposed development, would be deemed to comply with the requirements of the *Environmental Protection (Noise) Regulations 1997* for the proposed hours of operation, with the inclusion of the following:

- 1 For the outdoor play area, a 1200mm high wall on the first floor as shown in Appendix A would be required.
- 2 Mechanical plant would need to be placed within the proposed plant.
- 3 No car bays would be required to be restricted during the “Night Period”.

Finally, it is recommended to adopt best practices in managing a child care centre to reduce noise, including but not limited to no music to be played outside, and favouring soft finishes in the outdoor play area.

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APPENDIX A

PLANS

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Childcare Calculations

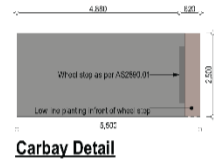
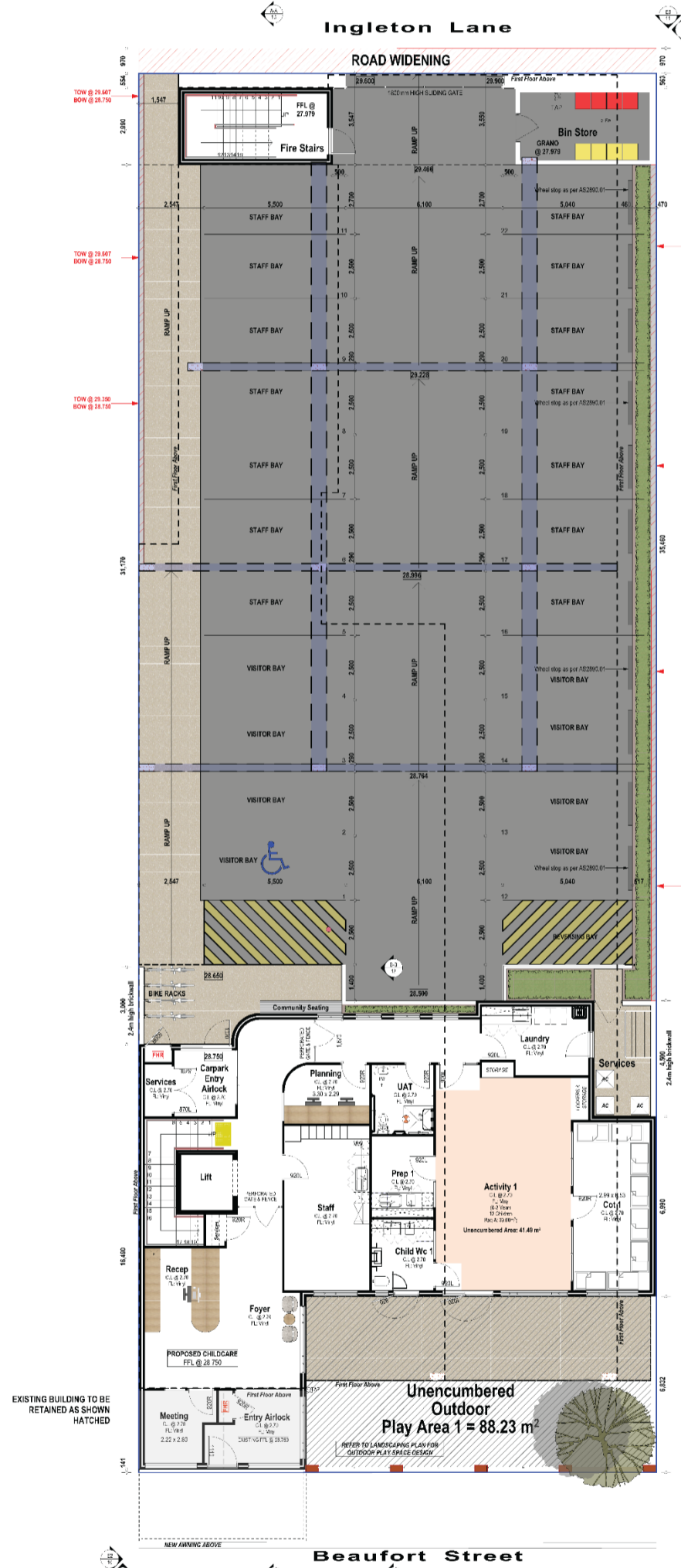
Room	Age (Yrs)	Quant.	Size	Staff Req
Activity 1	0-2	12	41.49m ²	3
Activity 2	2-3	10	40.29m ²	2
Activity 3	2-3	10	34.68m ²	2
Activity 4	3+	20	65.78m ²	2
Activity 5	3+	20	43.07m ²	2
Atelier			22.62m ²	
Piazza			43.07m ²	
				2

Total Internal = 72 **291.007m²** 13
(Min 3.25m² per child) (Min 234.00m² req)

Total External Play Area = 72 **515.06.86m²**
(Min 7m² per child) (Min 504.00m² req)

City of Stirling Parking Calculations

Description	Req	Provided
1 bay per 7 child and 1 per staff		
Child Bays	11	8
Staff Bays	13	13
	24	21



Zone	Area	Perim
Site Foot	17.34	17.653
Excavate	432.25	1210.62
Causeway	483.22 m ²	1852.47 m

Ground Floor	Area	Perim
Services	16.35	13.843
Sh. Bays	16.12	10.783
Fire Stairs	17.34	17.583
Causeway	232.30	734.639
	297.29 m ²	1012.89 m
	171.85 m ²	502.89 m

Site Calculations	Value
Site Area	1,139 m ²
Building Footprint	273.32 m ²
Site Coverage	24.02%
Allowable Site Coverage	N/A%

Attribute	Value
Structure	RM
Use	Mixed Use
Use Group	RM
Structure	RM
SAL	NA
Access	TBC
Roof	High/Low
Power	Above
Coastline	RM
Other	TBC
Wind	100

Project Name: Childcare Centre
Project Address: Lot 112 (M752) Beaufort St, Mt Lawley
Drawing Title: Ground Floor Plan
Scale: 1:100
Project No: 22082
Revision Number: 6.00
Drawing No: 08 of 14



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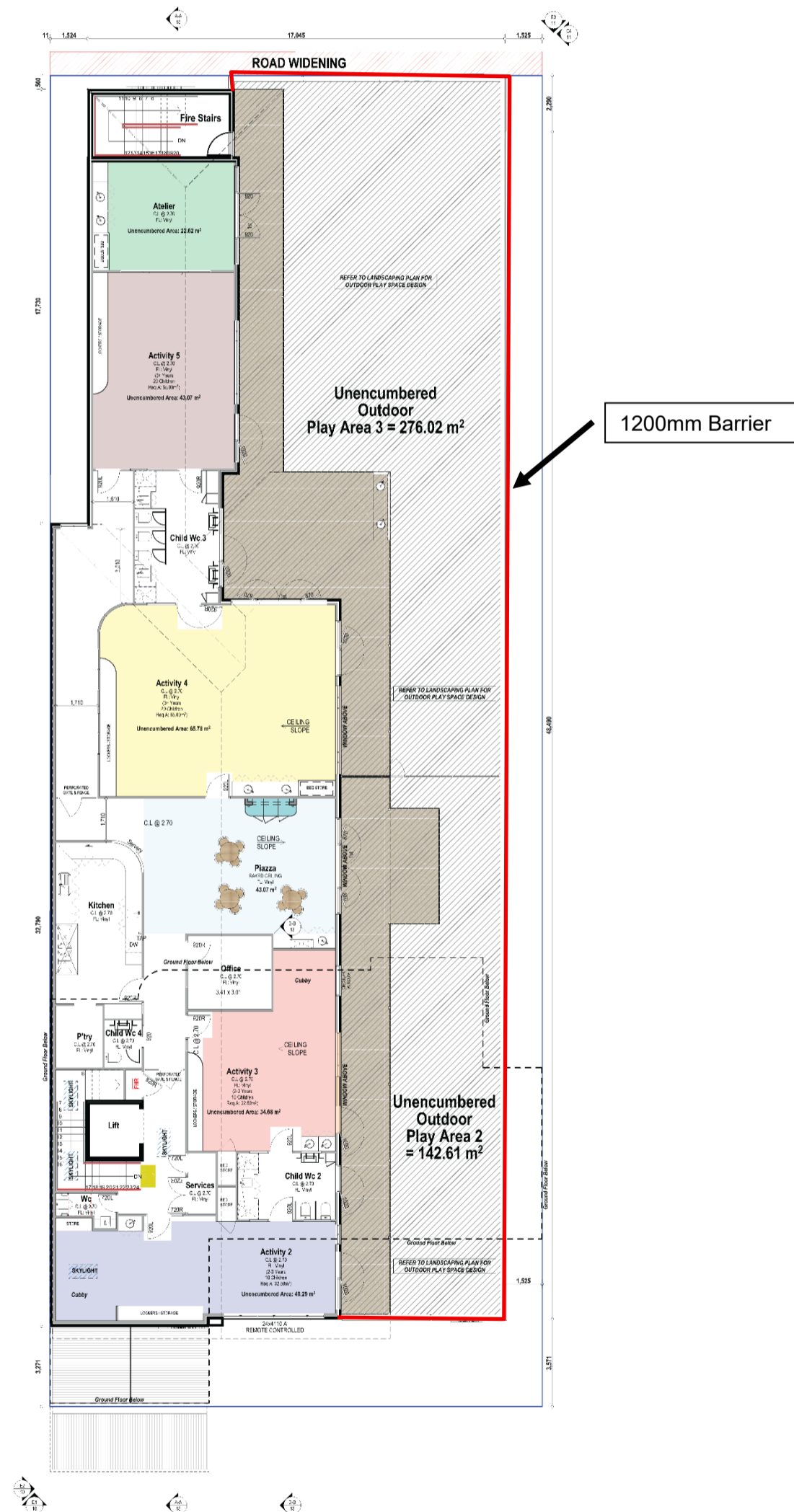
Childcare Calculations

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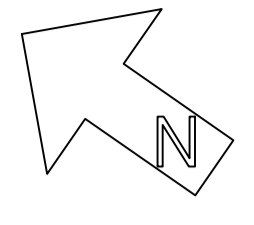


Revision	Description	Date
010	Development Approved	04.02.24
020	Finalize Comments	17.02.24
030	Final Comments	02.03.24
040	DSR	10.03.24
050	Changes	04.04.24
060	Changes	24.04.24

Project Name: Childcare Centre
Project Address: Lot 112 (4752) Beaufort St, Mt Lawley
Drawing Title: First Floor Plan
Scale: 1:100 Sheet Size: A1
Project No: 22082 Revision Number: 6.00
Drawing No: 09 of 14



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EXISTING BUILDING TO BE RETAINED AS SHOWN HATCHED



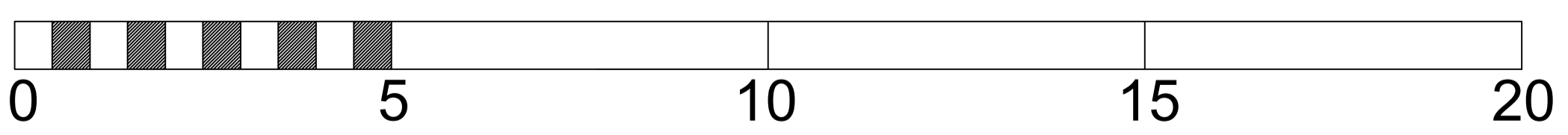
Beaufort Street

Ingleton Lane

- TEEPEE
- LIMESTONE WALL
- SMALL FORT WITH SLIDE AND STAIRS
- SAND KITCHEN

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5.0m



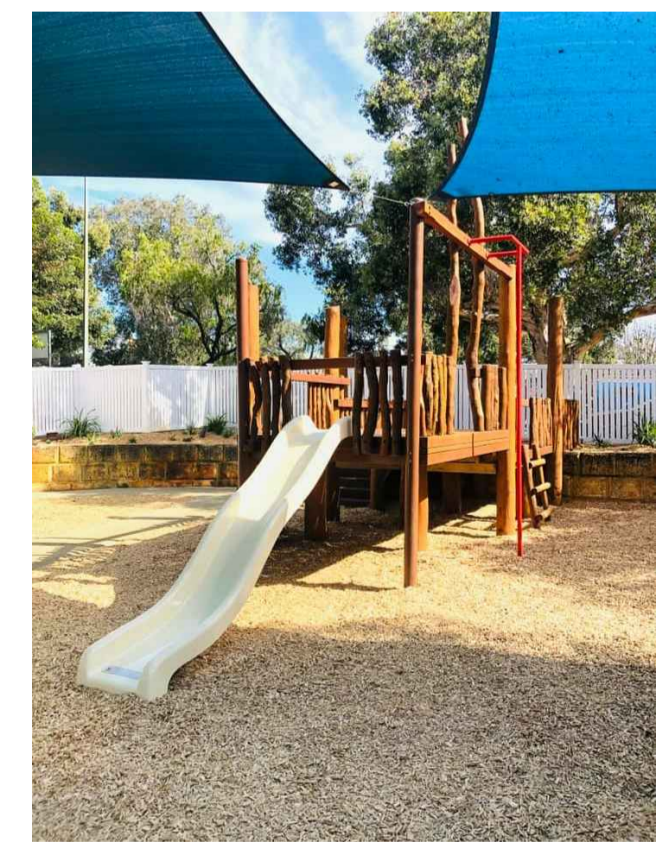
Note: While all effort has been taken to ensure the design is safe, this is a concept design only. It is the responsibility of the construction/ landscape contractor to ensure the playground meets playground safety standards




TEEPEE



SAND KITCHEN



SLIDE

 <p>P.O. Box 774 e-mail: admin@childscapes.com.au web: childscapes.com.au</p>	<p>PROJECT 752 BEAUFORT ST MOUNT LAWLEY ground floor</p>	<p>DRAWING OVERALL CONCEPT PLAN</p>				<p>ISSUE ISSUED FOR FEEDBACK</p>		<p>REVISIONS</p> <table border="1"> <thead> <tr> <th>No</th> <th>DATE</th> <th>DRAWN</th> <th>DETAILS</th> </tr> </thead> <tbody> <tr> <td>0</td> <td>07/11/25</td> <td>NK</td> <td>CONCEPT- ISSUED FOR FEEDBACK</td> </tr> <tr> <td>1</td> <td>23/03/26</td> <td>NK</td> <td>CONCEPT- ISSUED FOR FEEDBACK</td> </tr> </tbody> </table>		No	DATE	DRAWN	DETAILS	0	07/11/25	NK	CONCEPT- ISSUED FOR FEEDBACK	1	23/03/26	NK	CONCEPT- ISSUED FOR FEEDBACK
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