

# Karrinyup Activity Centre – Online Consultation



**23 February 2022 – 11 March 2022**

# Why planning is required now

The Karrinyup Shopping Centre has been the subject of significant redevelopment.

The Metropolitan North-West Joint Development Assessment Panel has also approved large-scale residential development on the shopping centre site.

State and Local Government planning strategies seek to consolidate commercial uses and higher-density housing in and around activity centres.

Parts of the local community have raised issues with the scale of recently approved development and is concerned that a similar scale and intensity of development could occur elsewhere in and around the Karrinyup Activity Centre.

This project is seeking to update the planning framework for the Karrinyup Activity Centre.



Recent development and approvals

# The engagement process so far

The local community has been continuously engaged in the planning process for the Karrinyup Activity Centre including:

- Approximately 95 local residents and business owners participating in a visioning workshop on 27 October 2021 to set a vision for the Karrinyup Activity Centre.
- The establishment of a Community Reference Group with 15 community members to provide oversight and guidance on the planning process.
- Approximately 71 local residents and business owners participated in a design workshop on 8 December 2021 to comment on planning and design ideas.
- The City is now consulting on a draft plan, which will be advertised from 23 February 2022 until 11 March 2022 with material posted on the City's website and provided in hard copy at City's premises to gain feedback from the local community.
- Further opportunities for community engagement will occur when a [Planning Scheme Amendment](#) and a Local Development Plan for the Karrinyup Activity Centre are advertised for public comment.



Completed

**Vision workshop 1**

**Design workshop 2**

Online Consultation

Council Approval for formal advertising of Council Planning Framework

Yet to be Completed

**Advertising of Planning Framework**

**Final Approval of Planning Framework**

# Proposed Objectives

The following draft objectives are proposed to provide over-arching guidance for new development within the Karrinyup Activity Centre:

**Objective 1:** Define a boundary to the Karrinyup Activity Centre to establish a frame of new development around the shopping centre.

**Objective 2:** Create a setting for employment-generating uses and medium-density housing.

**Objective 3:** Ensure that local streets and Karrinyup Road prioritise walking, cycling and public transport use over private motor vehicles.

**Objective 4:** Ensure the frame of new development around the shopping centre establishes a neighbourly transition in height from the shopping centre to the surrounding residential area.

**Objective 5:** Establish streetscapes that provide a desirable context for small business and apartment living and place a strong emphasis on the amenity of pedestrians and cyclists.

**Objective 6:** Locate on-site car parking so that it does not diminish the quality or amenity of the streetscape.

**Objective 7:** Ensure new development addresses the street with active ground level frontage (windows, doors, balconies, etc) and avoids the use of blank walls or fences.

**Objective 8:** Ensure the provision of high-quality landscaping designed to reinforce the landscape garden qualities of the existing place as an integral part of any new development.

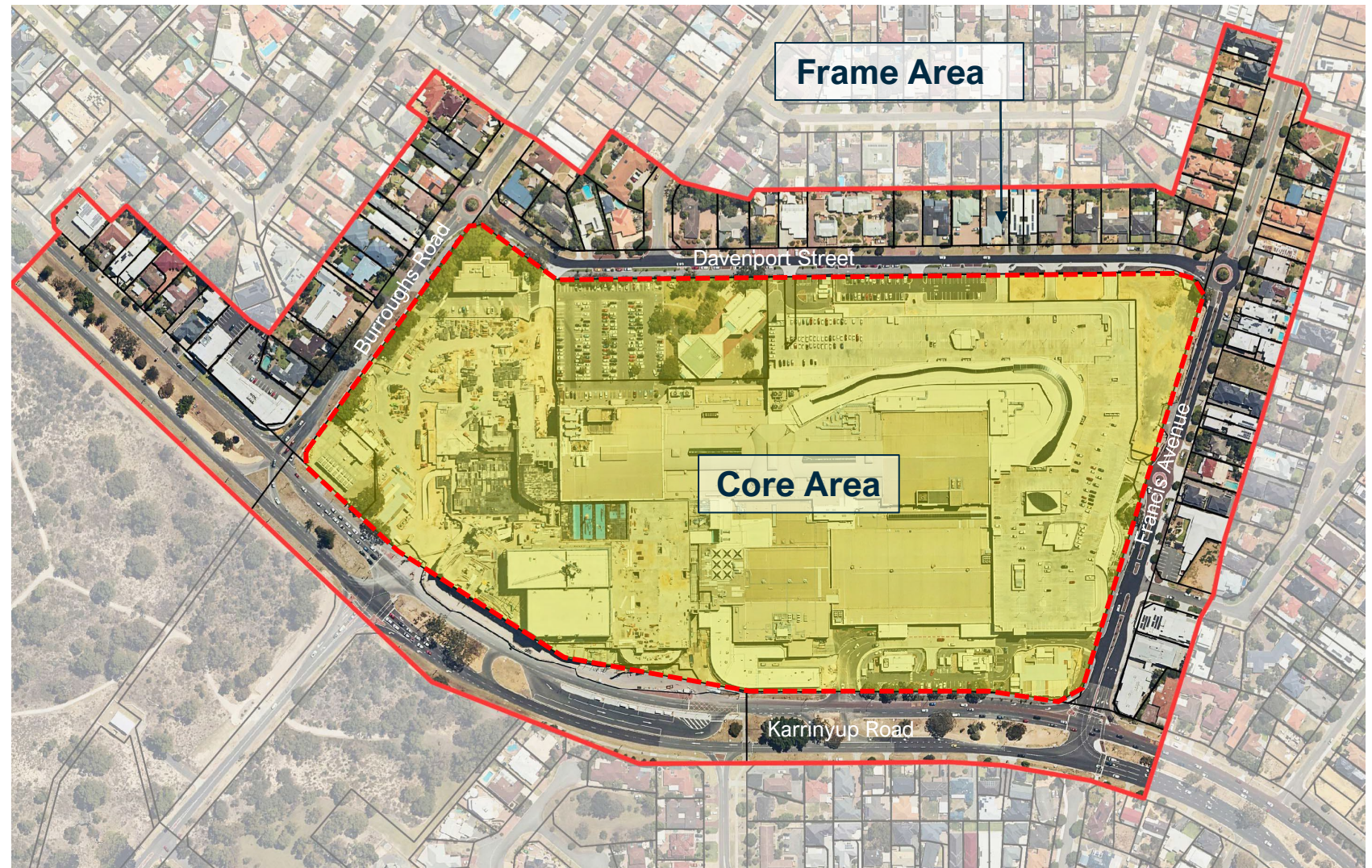
**Objective 9:** Establish a planning framework for the Karrinyup Activity Centre that controls the heights and bulk of buildings to be in keeping with the general low scale development of the surrounding residential areas.



# Proposed Boundary of the Karrinyup Activity Centre

The rationale for the proposed boundary of the Karrinyup Activity Centre is outlined below:

- The rear boundaries of the properties that face the shopping centre generally define the extent of the precinct.
- This ensures that the focus of activity remains on the streets that immediately surround the shopping centre and does not extend to the quieter residential streets beyond.
- The extension of the Activity Centre boundary across Karrinyup Road is limited because the width of Karrinyup Road diminishes the relationship between the two streets.
- The Activity Centre boundary has been extended marginally along Francis Avenue, Karrinyup Road and Burroughs Road to terminate at a street intersection consistent with good urban design practice.







# Proposed Land Use

The proposed land uses are outlined below:

The Karrinyup Activity Centre is proposed to be divided into four land use zones – ‘Regional Centre’ (as existing), ‘Development’ (as existing), ‘Centre’ and ‘Residential (R-AC0)’.

**Proposed Preferred Uses:** Detailed guidance on permitted and excluded land uses is proposed to be provided through an amendment to the City of Stirling Local Planning Scheme No.3. In general, the preferred uses for each of the zones are:

-  **Regional Centre:** Retail, food and beverage, office, residential, personal and business services, short-stay accommodation.
-  **Residential:** Residential uses.
-  **Centre Zone:** Residential, office, medical/consulting, childcare, personal and business services, small-scale shops, small-scale food and beverage, short-stay accommodation.
-  **Development:** Land use subject to future planning of the site.

**Proposed Excluded Uses:** The following types of land uses are proposed not to be permitted in the Centre Zone: Drive-through fast food, showrooms, large-scale shops, motor vehicle repair, taverns, hotels and nightclubs.



# Proposed Building Types

The following proposed buildings types have been developed through input from the community at the workshops.

## Residential Building Types

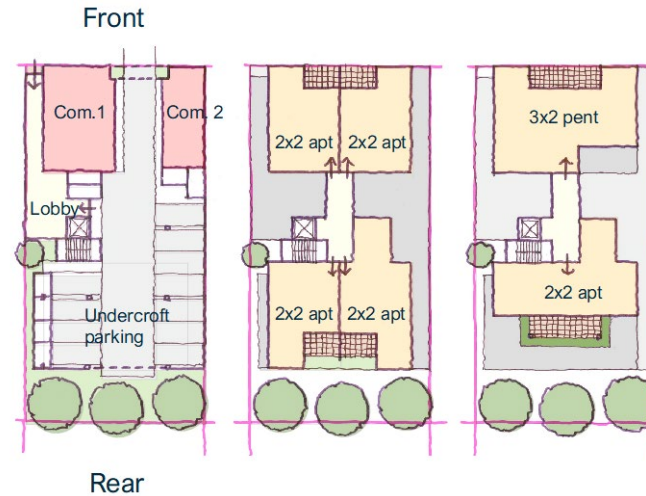
- Three-storey detached development with deep soil zones on all boundaries for landscaping and tree canopy.
- Buildings setback from the street with windows, doors, balconies and landscaped courtyards facing the street.
- At the rear dwellings to face the large rear deep soil areas that provide a landscaped outlook.
- Grouped housing is proposed to be permitted provided that they fit in with the building envelopes.

## Centre Building Types

- To be built to the street boundary with awnings to create an attached 'shopfront' with a range of commercial uses that can be incorporated.
- Creation of a pedestrian friendly environment along the streets.
- On-site car bays are proposed to be located under and behind the building to avoid diminishing the quality of the streetscape.

Any development in the 'Regional Centre' zone is proposed to be subject to the City's [Mixed Use Design Guidelines](#).

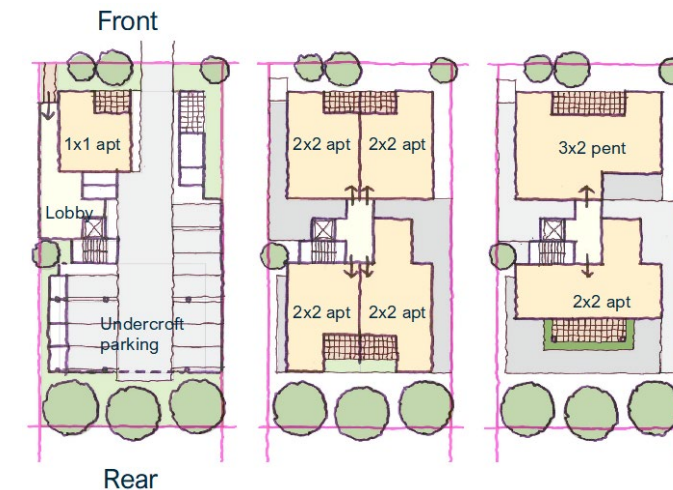
## Centre Building Types



An example of mixed-use development already within the precinct



## Residential Building Types

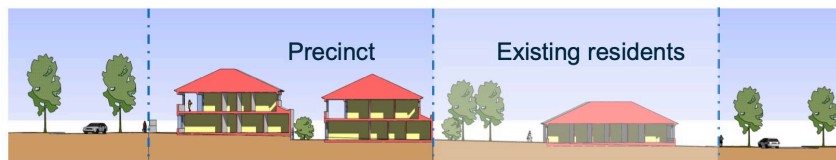


# Proposed Development Controls – Frame Area

The proposed development controls are to be included in a Local Development Plan for the Karrinyup Activity Centre. Proposed permitted and excluded land uses would be included in an amendment to the City's [Local Planning Scheme](#).

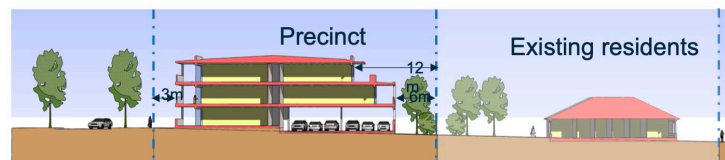
Development Standards	Centre Type Buildings	Residential Type Buildings
Maximum Height	3 storeys max	3 storeys max
Front Setback	Nil max	3m min
Side Setbacks	Nil min	3m min
Rear Setback (ground and 1 <sup>st</sup> )	6m min	6m min
Rear Setback (2 <sup>nd</sup> )	12m min	12m min
Deep Soil Areas	15% min	20% min

- Car parking requirements proposed to be as per R-Codes and Local Planning Scheme.
- Consistency with the relevant provisions of the R-Codes, Local Planning Scheme and Local Planning Policies.



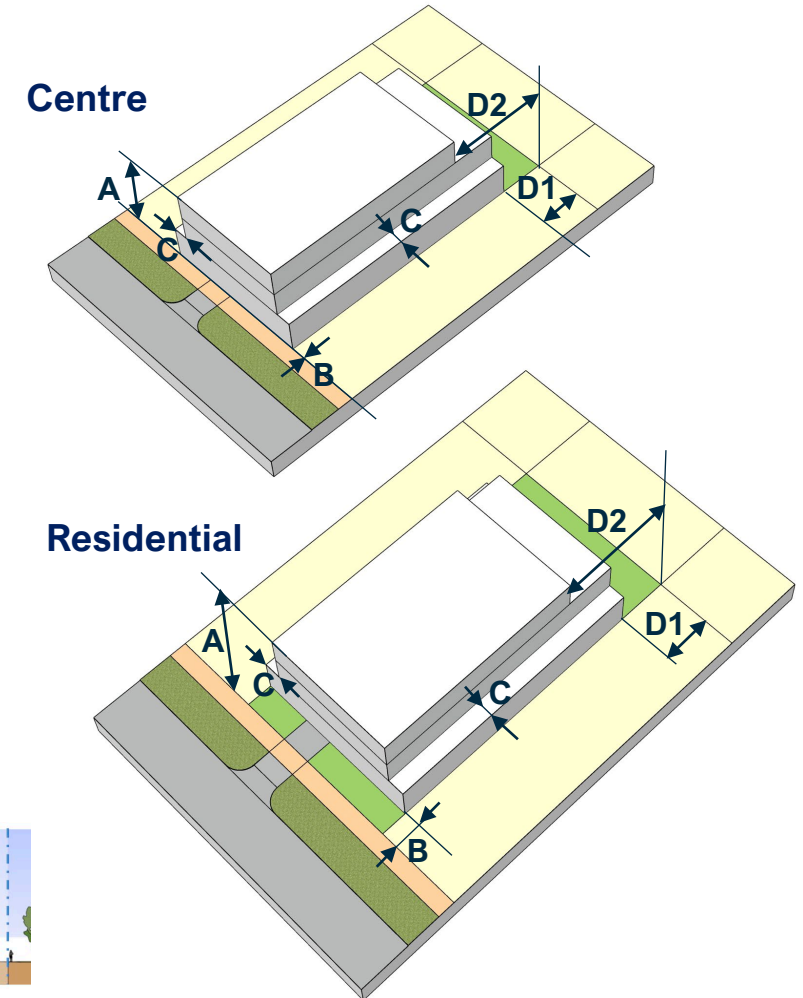
Status quo – duplex development

At present, two-storey development can be built close to the rear boundary.



Small-scale apartment development

Proposed LDP pushes new development away from the rear boundary to provide for trees.



# Proposed development controls for built form – Core Area

The following draft provisions are proposed to control development within the core area.

## Development controls for the ‘Development’ zone (City of Stirling Land – Library Site)

Further planning is required for this site and is subject to a future project if the site is redeveloped.

## Development controls for the ‘Regional Centre’ zone

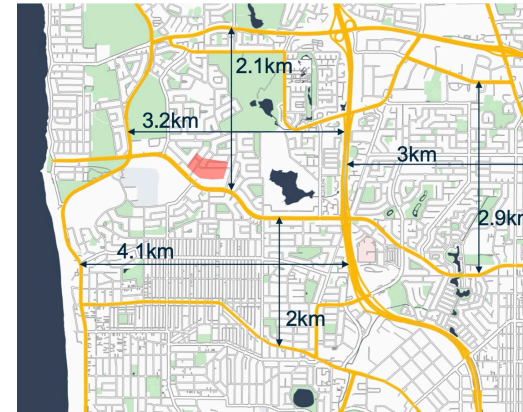
Proposed to comply with the City's [Mixed Use Design Guidelines](#).

Development over 3 storeys would require a separate Local Development Plan.

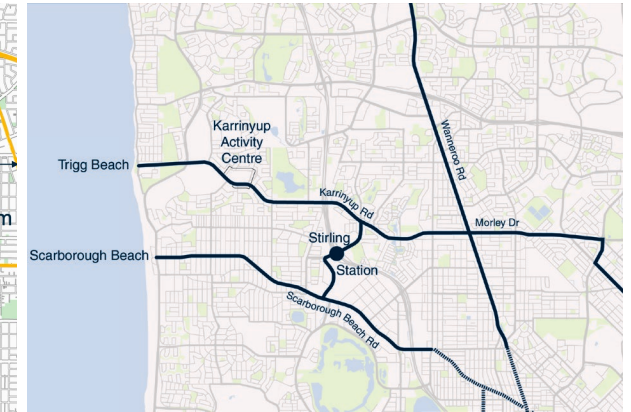
# Transport: Overview of transport context and issues

Karrinyup is not an easy place to get to. It has no railway station, and the major roads are too far apart for traffic to move efficiently. Therefore, the only sensible alternatives are to limit traffic demand by limiting the extent of new development in the centre or accommodating alternative means of transport.

The State Government has responsibility for regional transport. The City of Stirling and its residents have the capacity to advocate for a more balanced transport outcome. In the meantime, the current car-based context is a compelling planning reason to limit development within the Karrinyup activity centre.



The spacing of the regional road network is greater than the recommended 1.6km.



The City of Stirling has a vision for a light rail network to connect with the rail network.



**Business as usual:** A transport strategy based on accommodating traffic will result in Karrinyup Road being widened at the expense of existing trees and the amenity of residents who live there.



**Proposed future:** A balanced transport strategy could result in Karrinyup Road accommodating light rail, a bike boulevard in a landscaped median, and street trees to provide shade for pedestrians.

# Transport: Proposed Cross-Sections

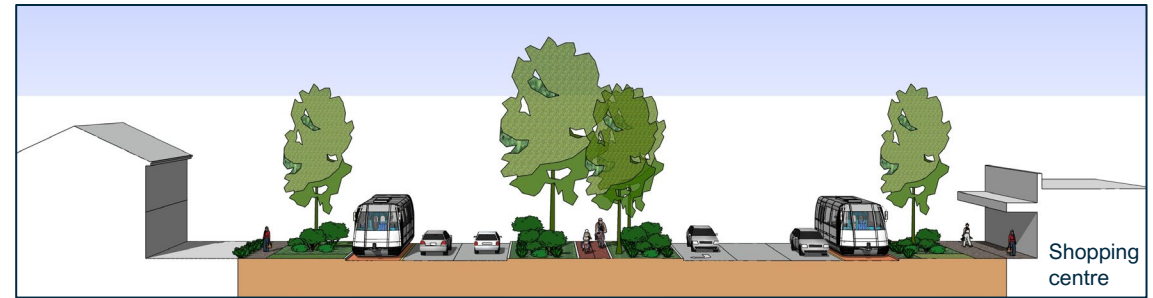
The following draft cross sections are proposed for the centre on four streets – Karrinyup Road, Francis Avenue, Burroughs Road and Davenport Street. It is proposed that these roads would be upgraded by developers at the time of redevelopment of adjoining properties.

**Karrinyup Road** is an arterial road that performs a regional function. It has sufficient width to accommodate significant landscaping and a future light rail.

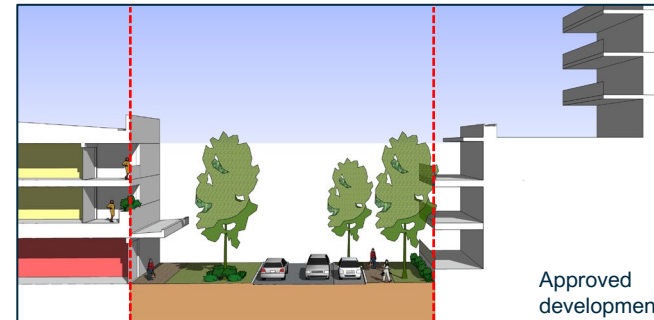
**Francis Avenue** distributes traffic from Karrinyup Road into the Activity Centre and the suburb of Karrinyup. The western side of Francis Avenue is mostly determined and built.

**Burroughs Road** distributes traffic from Karrinyup Road into the centre and the suburb of Karrinyup. Development on the western side of Francis Avenue was recently approved but not yet built.

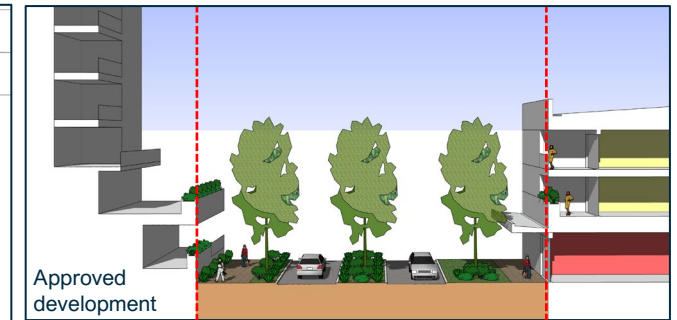
**Davenport Street** is a local road that connects Francis Avenue to Burroughs Road. The southern side of Davenport Street has been designed and built but has scope for improved landscape and enhanced pedestrian connectivity.



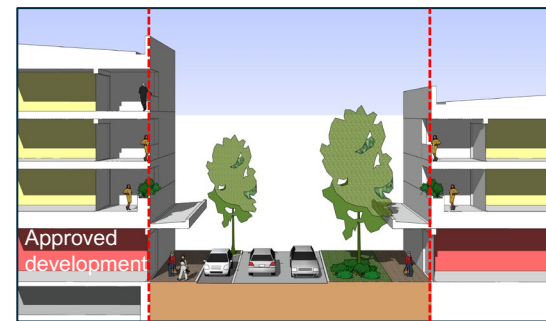
Karrinyup Road – controlled by the State Government



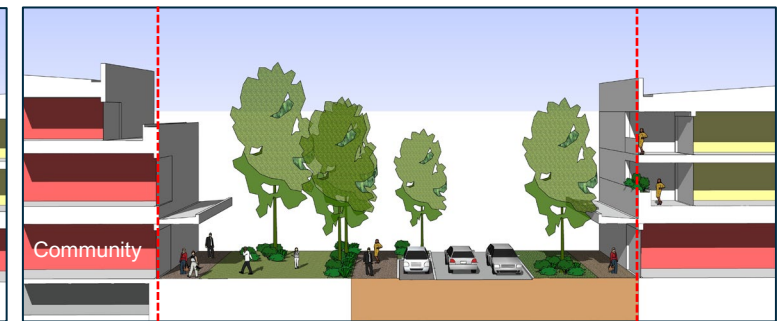
Burroughs Road - looking north



Francis Avenue - looking north



Davenport Street - shopping centre looking west



Davenport Street - community hub looking west

# Proposed Pedestrian and Cyclist Network

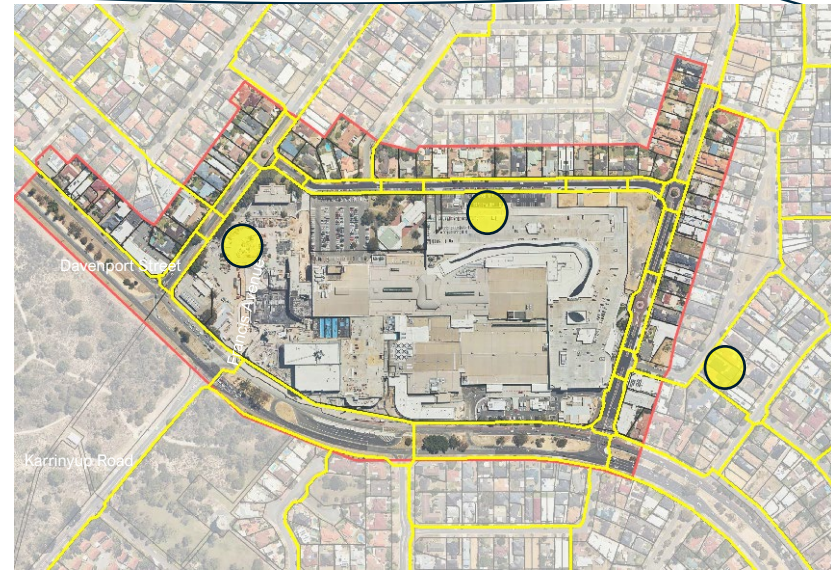
Traditionally, shopping centres are designed as car-orientated destinations. This negatively impacts the people who live nearby.

Now, pedestrians and cyclists, living within walking and cycling distance of centres are increasingly being recognised as important.

The first step to encourage people to walk and cycle to Karrinyup Activity Centre, is to identify **key pedestrian** and **cycle routes** (shown on adjacent maps).

The redevelopment of the Karrinyup Shopping Centre has already provided some improvements to this network, including new footpaths and shared cycle paths. Additional improvements could include:

- New Footpaths (to be delivered in next 12 months paid for by the shopping centre);
- New Bike lanes/shared pathways (funded by shopping centre);
- New Awnings (provided by developers);
- New Street trees (in accordance with the City's Tree planting programme); and
- Possible upgraded pedestrian crossing points.



**Key pedestrian routes** (paths leading to and around the centre core)



**Key cycle routes**

# Possible Streetscapes

The following perspectives illustrate how the local streets may look once all development is completed and roads have been upgraded by developers in accordance with the proposed cross sections.



**Burroughs Road**



**Davenport Street**



**Francis Avenue**

# Community Infrastructure: Stirling Libraries - Karrinyup Site

The planning process for the Karrinyup Activity Centre is an ideal opportunity to consider the future of the City of Stirling library site, most of which is an at-grade car park largely used by patrons of the library, community centre and the shopping centre.

There was strong support amongst workshop participants for the provision of a new library and other complimentary facilities.

The adjacent concept plan suggests how the City's land could be redeveloped to locate the parking in a basement, provide a new library and complimentary facilities integrated with other commercial uses to help fund it. The development could create a new town square whilst retaining existing trees, and create a network of landscaped pedestrian lanes.

This approach is consistent with prevailing trends elsewhere in Australia that seek to integrate community facilities and spaces into a more cohesive town centre rather than being a stand-alone and drive-to facility.

The delivery of such a concept will be subject to further detailed planning in consultation with the community and agreement from Council.

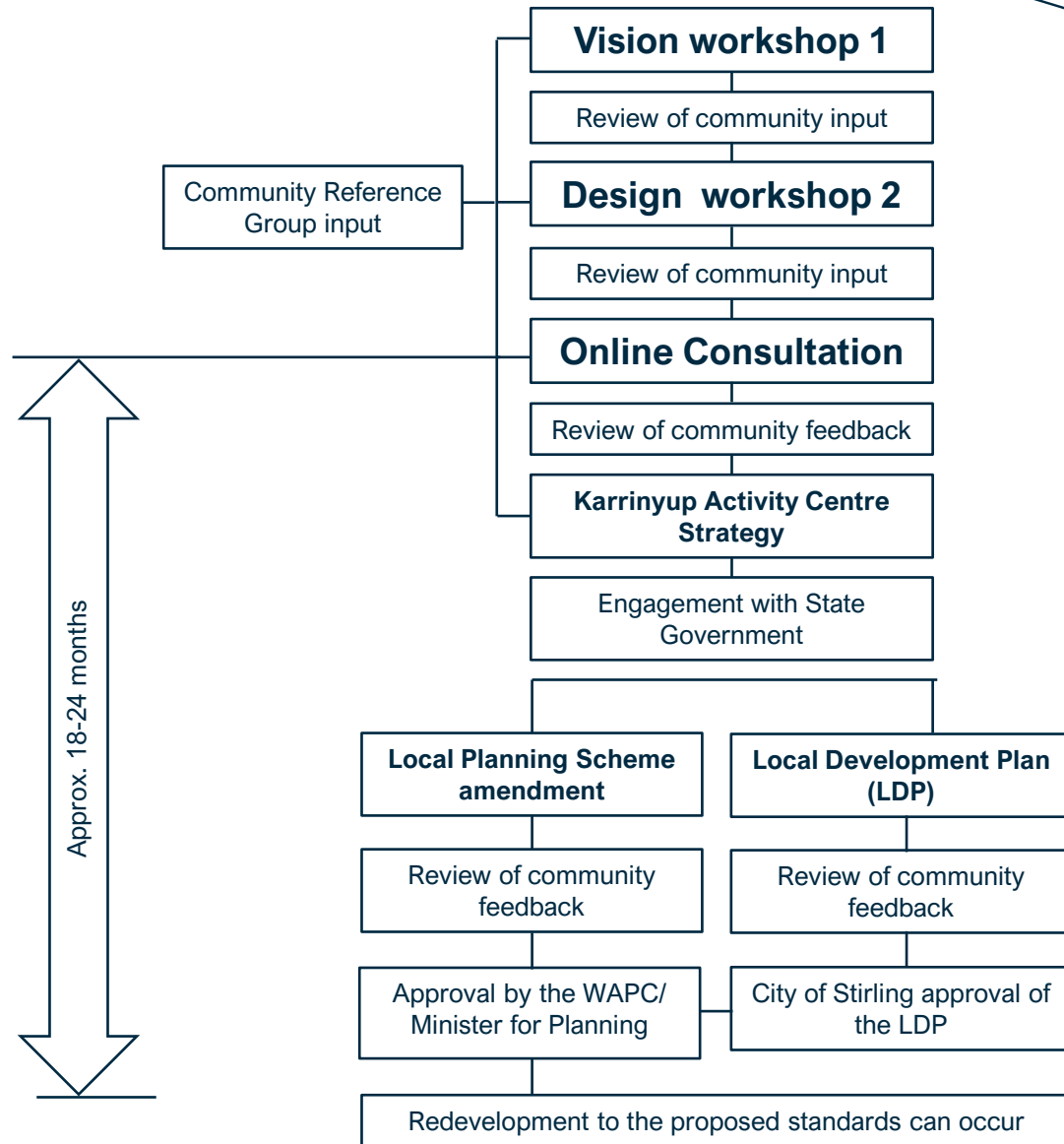


1. New library and other complementary facilities in a village of buildings and lanes.
2. Reconfiguration of the existing green space to retain the trees in a new town square.
3. Generous setback from Davenport Street to enable an avenue of trees.
4. Parking in a new basement car park.
5. Pedestrian street connecting to the 'main street'.
6. Retail 'sleeve' to hide the blank wall to Myer.

# Implementation: Next steps

To make it happen, the following is proposed to occur:

- Prepare a Karrinyup Activity Centre strategy document that describes this planning process, the outcomes and the steps necessary to deliver the outcomes.
- Engage with State Government to ensure that the proposed outcomes are consistent with State planning processes, strategies and infrastructure planning.
- Prepare, advertise, and approve a Local Development Plan that provides the primary development controls for new development.
- Prepare an amendment to the Local Planning Scheme that rezones land within the Karrinyup Activity Centre and, potentially, places key development controls within the scheme to limit the exercise of discretion of decision makers.
- Liaise with State Government regarding improvements to the regional transport network.



## Feedback: How to respond

Your feedback on the proposed planning initiatives for the Karrinyup Activity Centre are welcome and will help to refine the outcomes and create a better Karrinyup.

The Open Day material and feedback survey is available online from the 23 February 2022, online by visiting <https://www.stirling.wa.gov.au/KAC>, or scanning the QR code, and on request at Stirling Libraries – Karrinyup and the City Administration Building, 25 Cedric Street, Stirling.

The closing time for feedback is 5pm 11 March 2022.

Thank you.

